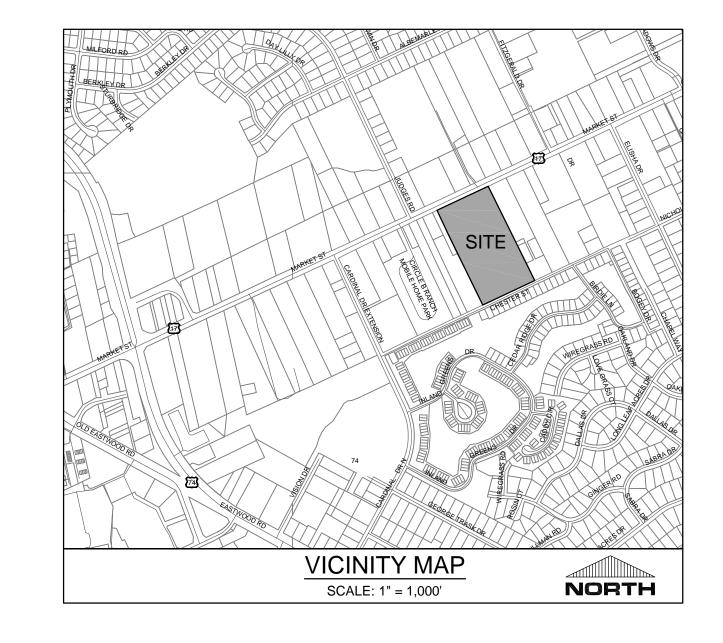


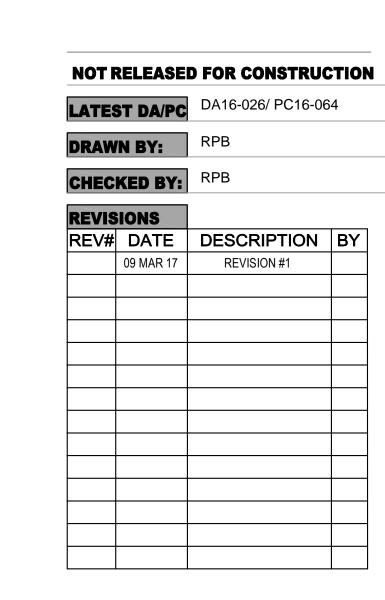
CARMAX OF WILMINGTON

WILMINGTON, NORTH CAROLINA

NOVEMBER 2016

PROJECT ADDRESS: 6030 MARKET STREET Wilmington, NC 28403







20-15081.02 11 NOV 16 SHEET TITLE **COVER SHEET**

CARMAX AUTO SUPERSTORE, INC. 12800 TUCKAHOE CREEK PARKWAY

RICHMOND, VA 23238

CIVIL ENGINEERS, LAND PLANNERS & LANDSCAPE ARCHITECTS:

PARAMOUNTE ENGINEERING, INC. 122 CINEMA DRIVE WILMINGTON, NORTH CAROLINA 28403 ATTN: ROB BALLAND (910) 791-6707

SHEET INDEX				
SHEET NUMBER	SHEET TITLE			
C-0.0	COVER SHEET			
C-2.0	DEMOLITION PLAN			
C-2.1	SITE PLAN			
C-2.2	SITE INVENTORY MAP			
C-2.3	TREE REMOVAL PLAN			
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C-6.0 - C-6.1, C-6.8 - C-6.9	DETAILS			
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NOTICE REQUIRED

ALL EXISTING UNDERGROUND UTILITIES SHALL BE PHYSICALLY LOCATED PRIOR TO THE BEGINNING OF ANY CONSTRUCTION IN THE VICINITY OF SAID UTILITIES.

CONTRACTORS SHALL NOTIFY OPERATORS WHO MAINTAIN UNDERGROUND UTILITY LINES IN THE AREA OF PROPOSED EXCAVATION AT LEAST TWO WORKING DAYS, BUT NOT MORE THAN TEN WORKING DAYS PRIOR TO COMMENCEMENT OF EXCAVATION OR DEMOLITION.

CONTACT "CAROLINA ONE CALL" AT

1-800-632-4949

CONTACT THESE UTILITIES

CITY OF WILMINGTON, DEVELOPMENT SERVICES PH: 910-341-3257

PIEDMONT NATURAL GAS ATTN: CARL PAQUET PH: 910-350-2242

EMERGENCY DIAL 911

POLICE - FIRE - RESCUE

CAPE FEAR PUBLIC UTILITY AUTHORITY (WATER & SEWER)

PH: 910-332-6670 DUKE ENERGY PROGRESS

PH: 910-602-4304 **BELL SOUTH**

ATTN: KEVIN LEATHERWOOD

ATTN: STEVE DAYVAULT

PH: 910-392-8712

PH: 910-763-4638

TIME WARNER CABLE

PROJECT NO. Public Services • Engineering Division APPROVED STORMWATER MANAGEMENT PLAN For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy Approved Construction Plan and/or project acceptance. NCDENR PWSS WATER PERMIT #: WATER CAPACITY: DWQ SEWER PERMIT #: SEWER CAPACITY: SEWER SHED # AND PLANT: SEWER TO FLOW THROUGH NEI: YES or NO (CIRCLE ONE)

1. THE CONTRACTOR IS REQUIRED TO OBTAIN ANY/ALL PERMITS REQUIRED FOR CONSTRUCTION OF THESE PLANS.

- ALL CONSTRUCTION TO BE IN ACCORDANCE WITH PERMITS ISSUED AND WITH THE CITY OF WILMINGTON, NEW HANOVER COUNTY, CAPE FEAR PUBLIC UTILITY AUTHORITY (CFPUA), AND THE STATE OF NORTH CAROLINA.
- THE CONTRACTOR IS TO ESTABLISH AND CHECK ALL HORIZONTAL AND VERTICAL CONTROLS TO BE USED WITH THE PROJECT. IN ADDITION, THE CONTRACTOR IS TO COMPUTE THE LAYOUT OF THE ENTIRE SITE PLAN IN ADVANCE OF BEGINNING ANY WORK ASSOCIATED
- WITH THE SUBJECT PLANS. CONTRACTOR SHALL EMPLOY A PROFESSIONAL SURVEYOR TO PERFORM SITE IMPROVEMENT STAKEOUT(S). ANYTIME WORK IS PERFORMED OFF-SITE OR WITHIN AN EXISTING EASEMENT, THE CONTRACTOR IS TO NOTIFY THE HOLDER OF SAID EASEMENT AS TO THE NATURE OF PROPOSED WORK, AND TO FOLLOW ANY GUIDELINES OR STANDARDS WHICH ARE ASSOCIATED WITH OR
- 5. CONTRACTOR SHALL REFER TO ARCHITECTURAL DRAWINGS BY OTHERS FOR ALL BUILDING DIMENSIONS AND DETAILS.

GENERAL NOTES

REFERENCED IN THE RECORDED EASEMENT.

- EXISTING CONDITIONS AND TOPOGRAPHIC SURVEY COMPLETED BY PARAMOUNTE ENGINEERING, INC. THE SURVEY SHALL BE FIELD VERIFIED BY CONTRACTOR AND ANY DISCREPANCIES REPORTED TO THE OWNER, OWNERS REPRESENTATIVE AND/OR ENGINEER
- REASONABLE CARE HAS BEEN EXERCISED IN SHOWING THE LOCATION OF EXISTING UTILITIES ON THE PLANS. THE EXACT LOCATION OF ALL EXISTING UTILITIES IS NOT KNOWN IN ALL CASES. THE CONTRACTOR SHALL EXPLORE THE AREA AHEAD OF DITCHING OPERATIONS BY OBSERVATIONS, ELECTRONIC DEVICES, HAND DIGGING AND BY PERSONAL CONTACT WITH THE UTILITY COMPANIES. IN ORDER TO LOCATE EXISTING UTILITIES IN ADVANCE OF TRENCHING OPERATIONS SO AS TO ELIMINATE OR MINIMIZE DAMAGE TO EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS RESULTING FROM ANY DAMAGE TO THE EXISTING UTILITY LINES INCLUDING LOSS OF UTILITY REVENUES. CONTRACTOR SHALL ARRANGE FOR TEMPORARY SUPPORT OF EXISTING UTILITIES, SUCH AS POLES, CONDUITS, FIBER OPTIC CABLES, TELEPHONE CABLES, WATER LINES, ETC.
- CONTRACTOR SHALL COMPLY WITH THE LATEST REVISIONS AND INTERPRETATIONS OF THE DEPARTMENT OF LABOR SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION PROMULGATED UNDER THE OCCUPATIONAL SAFETY AND HEALTH ACT.
- 4. CONTRACTOR SHALL PLAN AND CONSTRUCT WORK SO AS TO CAUSE MINIMUM INCONVENIENCE TO THE OWNER AND THE PUBLIC. THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN AT ALL TIMES DURING THE PROGRESS OR TEMPORARY SUSPENSION OF WORK, SUITABLE BARRIERS, FENCES, SIGNS OR OTHER ADEQUATE PROTECTION, INCLUDING FLAGMEN AND WATCHMEN AS NECESSARY TO INSURE THE SAFETY OF THE PUBLIC AS WELL AS THOSE ENGAGED IN THE CONSTRUCTION WORK. CONSTRUCTION SIGNING SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF "CONSTRUCTION AND MAINTENANCE OPERATIONS SUPPLEMENT TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" BY THE DOT.
- ALL MATERIAL CLEARED OR DEMOLISHED BY THE CONTRACTOR IN ORDER TO CONSTRUCT THE WORK SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE PROPERLY DISPOSED OF OFF-SITE.
- 6. ALL WORK BY THE CONTRACTOR SHALL BE WARRANTEED BY THE CONTRACTOR FOR A PERIOD OF ONE YEAR AFTER THE OWNER
- CONTRACTOR SHALL CALL THE NORTH CAROLINA ONE-CALL CENTER AT 1 (800) 632-4949 AND ALLOW THE CENTER TO LOCATE EXISTING UTILITIES BEFORE DIGGING.

DEMOLITION NOTES:

- 1. CONTRACTOR TO COORDINATE WITH THE OWNER TO PROPERLY MAINTAIN OR RELOCATE EXISTING SERVICE CONNECTIONS WHEN
- CONTRACTOR IS TO WALK THE SITE AND BECOME FAMILIAR WITH THE SCOPE OF DEMOLITION REQUIRED. ALL DEMOLITION WORK REQUIRED TO CONSTRUCT NEW SITE IMPROVEMENTS WILL BE PERFORMED BY THE CONTRACTOR AND WILL BE CONSIDERED UNCLASSIFIED EXCAVATION.
- DEMOLITION SHALL INCLUDE BUT IS NOT LIMITED TO THE EXCAVATION, HAULING AND OFFSITE DISPOSAL OF CONCRETE PADS, CONCRETE DITCHES, FOUNDATIONS, SLABS, STEPS, AND STRUCTURES; ABANDONED UTILITIES, BUILDINGS, PAVEMENTS AND ALL MATERIALS CLEARED AND STRIPPED TO THE EXTENT NECESSARY AS DIRECTED BY THE SOILS ENGINEER FOR THE INSTALLATION OF THE NEW IMPROVEMENTS AND WITHIN THE LIMITS OF CLEARING AND GRADING AND AS SHOWN ON THESE PLANS.
- THE CONTRACTOR SHALL PROTECT ALL ADJACENT PROPERTY, STRUCTURES AND UTILITIES ON THE PROPERTY NOT TO BE DEMOLISHED. DAMAGE TO PROPERTIES OF OTHERS DUE TO THE CONTRACTOR'S ACTIVITIES SHALL BE REPLACED IN KIND BY THE CONTRACTOR AT NO
- 5. ELECTRIC, TELEPHONE, SANITARY SEWER, WATER AND STORM SEWER UTILITIES THAT SERVICE OFF-SITE PROPERTIES SHALL BE MAINTAINED DURING THE CONSTRUCTION PROCESS BY THE CONTRACTOR.
- THE CONTRACTOR SHALL PRODUCE A PHOTOGRAPHIC RECORD (DIGITAL) OF DEVELOPMENT COMMENCING WITH A RECORD OF THE SITE AS IT APPEARS BEFORE DEMOLITION HAS BEGUN. AFTERWARDS, A PHOTOGRAPHIC RECORD SHALL BE MAINTAINED WEEKLY DURING CONSTRUCTION AND ENDING WITH A PHOTOGRAPHIC RECORD OF THE DEVELOPMENT AS IT APPEARS AFTER DEMOLITION. THIS RECORD
- 7. EXISTING CURB AND GUTTER, LIGHTS, SIDEWALK, AND UTILITIES NOT INTENDED FOR DEMOLITION SHALL BE MAINTAINED, PROTECTED
- ALL EXISTING IMPROVEMENTS INDICATED OR REQUIRED TO BE DEMOLISHED SHALL INCLUDE REMOVAL FROM THE PROPERTY AND PROPER DISPOSAL
- CONTRACTOR SHALL COORDINATE RELOCATION OF ALL EXISTING OVERHEAD AND UNDERGROUND UTILITIES INCLUDING CABLE, GAS, TELEPHONE AND ELECTRIC AND ANY OTHER UTILITIES THROUGH THE SITE WITH THE RESPECTIVE COMPANIES.
- 10. PROVIDE SMOOTH SAW CUT OF EXISTING PAVEMENTS, CURBS AND GUTTERS AND SIDEWALKS TO BE DEMOLISHED.
- 11. ALL DEMOLITION WORK SHALL BE DONE IN STRICT ACCORDANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS AS WELL AS OSHA REGULATIONS.
- 12. EXISTING FIRE HYDRANTS ARE TO REMAIN IN SERVICE.
- 13. INFORMATION CONCERNING UNDERGROUND UTILITIES WAS OBTAINED FROM AVAILABLE RECORDS, BUT THE CONTRACTOR MUST DETERMINE THE EXACT LOCATION AND ELEVATIONS OF THE MAINS BY DIGGING TEST PITS BY HAND.

TRAFFIC NOTES

- 1. ALL PAVEMENT MARKINGS IN PUBLIC RIGHTS-OF-WAY & FOR DRIVEWAY(S) ARE TO BE THERMOPLASTIC & MEET NCDOT STANDARDS.
- TRAFFIC CONTROL DEVICES (INCLUDING SIGNS AND PAVEMENT MARKINGS) IN AREAS OPEN TO PUBLIC TRAFFIC ARE TO MEET MUTCD (MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES) STANDARDS.
- 3. ALL TRAFFIC CONTROL SIGNS AND MARKINGS NOT WITHIN THE PUBLIC RIGHT-OF-WAY ARE TO BE MAINTAINED BY THE PROPERTY OWNER IN ACCORDANCE WITH MUTCD STANDARDS.
- 4. ALL PARKING STALL MARKINGS AND LANE ARROWS WITHIN THE PARKING AREAS SHALL BE WHITE.
- ANY OPEN CUTTING OF A NCDOT STREET REQUIRES AN ENCROACHMENT PERMIT. IN CERTAIN CASES, AN ENTIRE RESURFACING OF THE AREA BEING OPEN CUT MAY BE REQUIRED.
- 6. TACTILE WARNING MATS TO BE INSTALLED AT ALL WHEELCHAIR RAMPS.
- 7. A LANDSCAPING PLAN, INDICATING THE LOCATION OF REQUIRED STREET- YARD PLANTINGS SHALL BE PROVIDED FOR REVIEW OF SIGHT DISTANCES
- 8. ALL SIGNS AND PAVEMENT MARKINGS IN AREAS OPEN TO PUBLIC TRAFFIC ARE TO MEET MUTCD STANDARDS.

EROSION CONTROL AND SEQUENCE OF CONSTRUCTION NOTES:

NOTE: THESE EROSION CONTROL AND SEQUENCE OF CONSTRUCTION NOTES ARE INTENDED FOR EACH "PHASE" OF CONSTRUCTION. THE ORDER AND STEPS TAKEN MUST BE IMPLEMENTED AS EACH PART OF THE PROJECT IS DEVELOPED; WHETHER AS A WHOLE OR IN PHASES. ANY EROSION CONTROL DEVICES/MEASURES MUST REMAIN IN PLACE UNTIL THE ENTIRE BASIN DESIGNED TO HANDLE SEDIMENTATION AND EROSION IS STABILIZED AND ALL IMPROVEMENTS WITHIN THE BASIN ARE COMPLETE.

- 1. CONSTRUCT TEMPORARY GRAVEL CONSTRUCTION ENTRANCE(S), ESTABLISH THE LIMITS OF DISTURBANCE, AND TEMPORARY SILT FENCE. 2. CLEAR AND REMOVE FROM SITE ALL TREES (ONLY ONES TO BE REMOVED), ROOTS, ROOT MAT, ETC. FROM THE AREA WITHIN THE
- 3. INSTALL REMAINING EROSION CONTROL MEASURES AS SHOWN ON THE PLANS WITHIN THE AREA DISTURBED. ALL EROSION CONTROL MEASURES MUST BE INSTALLED BEFORE COMMENCING CONSTRUCTION.
- 4. PLANT GRASS OVER ALL GRADED AREAS WITHIN 15 WORKING DAYS OF CEASE OF ANY GRADING ACTIVITY.
- PROTECTION TO STOP ANY SEDIMENT FROM ENTERING THE DRAINAGE SYSTEM.
- 6. IMMEDIATELY UPON THE INSTALLATION OF ANY STORM DRAINAGE SYSTEM, THE CONTRACTOR SHALL INSTALL OUTLET PROTECTION AS

IMMEDIATELY UPON THE INSTALLATION OF ANY STORM DRAINAGE CATCH BASIN, DROP INLET, ETC., THE CONTRACTOR SHALL INSTALL INLET

- SHOWN ON THE PLANS AND DETAILS.
- 7. THE CONTRACTOR SHALL CONVERT THE SEDIMENT BASIN TO THE WET DETENTION POND ONCE THE ENTIRE DRAINAGE BASIN IS FULLY STABILIZED. THE CONTRACTOR SHALL REMOVE ANY SEDIMENT FROM THE POND AND ESTABLISH THE FINAL POND BOTTOM ELEVATION. ONCE ALL CONSTRUCTION IS FINISHED WITHIN THE POND DRAINAGE AREA THE CONTRACTOR SHALL REMOVE THE SKIMMER TO COMPLETE
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING AND RESTORING TO PRE-CONSTRUTION CONDITIONS ANY AREAS OUTSIDE THE PROJECT LIMITS THAT MAY INADVERTENTLY BE DAMAGED DUE TO THE FAILURE OF THE EROSION CONTROL MEASURES.
- 9. DURING GRADING AND AFTER GRADING HAS BEEN COMPLETED, THE CONTRACTOR SHALL CONTINUE TO MAINTAIN PERMANENT AND TEMPORARY EROSION CONTROL MEASURES UNTIL FINAL APPROVAL.
- 10. UPON RECEIVING FINAL APPROVAL, THE CONTRACTOR CAN REMOVE TEMPORARY EROSION CONTROL MEASURES.
- 11. THE CONTRACTOR SHALL CONTINUE TO WATER, FERTILIZE, MOW AND MAINTAIN SPRIGGED, SODDED, AND PLANTED AREAS UNTIL ALL CONSTRUCTION IS COMPLETE.

GENERAL EROSION AND SEDIMENT CONTROL NOTES

- 1. THE EROSION CONTROL PLAN SHALL INCLUDE PROVISIONS FOR GROUNDCOVER ON ALL EXPOSED PERIMETER DIKES, SWALES, DITCHES, PERIMETER SLOPES AND ALL SLOPES STEEPER THAN 3:1 WITHIN 7 CALENDAR DAYS FROM THE LAST LAND DISTURBING ACTIVITY. GROUND COVER SHALL BE PROVIDED ON ALL OTHER DISTURBED AREAS WITHIN 14 CALENDAR DAYS FROM THE LAST LAND DISTURBING ACTIVITY.
- UNI ESS OTHERWISE INDICATED. ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE NORTH CAROLINA EROSION AND SEDIMENT CONTROL HANDBOOK. (NO SEPARATE PAYMENT).
- 3. THE CONTRACTOR SHALL NOTIFY PLAN APPROVING AUTHORITY ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITY, AND ONE WEEK PRIOR TO FINAL INSPECTION.
- 4. ALL EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE PLACED PRIOR TO CLEARING AND/OR LAND DISTURBANCE.
- 5. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN AND PERMIT SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- PRIOR TO COMMENCING LAND DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING, BUT NOT LIMITED TO OFF-SITE BORROW OR WASTE AREAS STAGING OR STORAGE AREAS). THE CONTRACTOR SHALL PREPARE AND SUBMIT A SUPPLEMENTARY EROSION CONTROL PLAN TO THE OWNER FOR REVIEW AND TO NEW HANOVER COUNTY FOR APPROVAL. CONTRACTOR SHALL PAY ALL FEES REQUIRED AND SHALL INSTALL NECESSARY MEASURES AT NO SEPARATE PAYMENT. THE CONTRACTOR SHALL PROVIDE THE OWNER AND THE ENGINEER A COPY OF THE AMENDED PERMIT.
- 7. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY EITHER THE REVIEWING AGENCY OR THE ENGINEER. (NO SEPARATE PAYMENT).
- 8. ALL DISTURBED AREAS ARE TO DRAIN TO APPROVED SEDIMENT CONTROL MEASURES AT ALL TIMES DURING LAND DISTURBING ACTIVITIES AND DURING SITE DEVELOPMENT UNTIL FINAL STABILIZATION IS ACHIEVED.
- 9. ALL AREAS DISTURBED BY CONSTRUCTION UNLESS OTHERWISE IMPROVED SHALL BE SODDED OR SEEDED AS INDICATED AND STABILIZED.

10. DURING DEWATERING OPERATIONS, WATER SHALL BE PUMPED INTO AN APPROVED FILTERING DEVICE PRIOR TO DISCHARGE TO RECEIVING

- GENERAL EROSION AND SEDIMENT CONTROL NOTES (CONT.)
- 11. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF-PRODUCING EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF THE EROSION CONTROL DEVICES SHALL BE MADE IMMEDIATELY.
- 12. ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED BY CONTRACTOR ONCE STABILIZATION OR A SUFFICIENT GROUND COVER HAS BEEN ESTABLISHED OR AS DIRECTED BY THE ENGINEER. (NO SEPARATE PAYMENT). NCDENR'S FINAL APPROVAL IS REQUIRED.
- 13. TEMPORARY GRAVEL CONSTRUCTION ENTRANCE SHALL BE REQUIRED AT ALL CONSTRUCTION STAGING AREA ENTRANCES AND ALL CONSTRUCTION ACCESS LOCATIONS INTO NON-PAVED AREA. (NO SEPARATE PAYMENT).
- 14. WHEN CROSSING CREEK OR DRAINAGE-WAY, THE DIVISION OF WATER QUALITY SHALL BE CONTACTED PRIOR TO DISTURBING A CREEK. THE CONTRACTOR SHALL INSTALL RIP-RAP WITH FABRIC ALONG DISTURBED BANKS AND CHANNEL AND RESTORE SLOPES TO ORIGINAL CONTOURS, BUT NOT STEEPER THAN 2:1 MAXIMUM. DISTURBED CREEK AREA SHALL BE STABILIZED IMMEDIATELY.

MAINTENANCE PLAN:

- ALL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CHECKED FOR STABILITY AND OPERATION FOLLOWING EVERY RUNOFF-PRODUCING RAINFALL BUT IN NO CASE LESS THAN ONCE EVERY WEEK. ANY NEEDED REPAIRS WILL BE MADE IMMEDIATELY TO MAINTAIN ALL PRACTICES AS DESIGNED.
- ALL CONSTRUCTION ENTRANCES WILL BE PERIODICALLY TOP DRESSED WITH AN ADDITIONAL 2 INCHES OF #4 STONE TO MAINTAIN PROPER
- SEDIMENT WILL BE REMOVED BEHIND THE SEDIMENT FENCE WHEN IT BECOMES 0.5 FEET DEEP AT THE FENCE. THE SEDIMENT FENCE WILL BE BE REPAIRED AS NECESSARY TO MAINTAIN A BARRIER. SILT FENCE STAKES WILL BE SPACED 6 FEET APART UNLESS A WIRE BACKING IS
- USED WITH 8 FOOT STAKE SPACING. ALL SEEDED AREAS WILL BE FERTILIZED, RESEEDED AS NECESSARY, AND MULCHED ACCORDING TO SPECIFICATIONS IN THE VEGETATIVE

DEPTH. ANY SEDIMENT THAT IS TRACKED INTO THE STREET WILL BE IMMEDIATELY REMOVED.

- INLET PROTECTION INSPECT WIRE AND ROCK INLET PROTECTION AT LEAST ONCE A WEEK AND AFTER EACH SIGNIFICANT (1/2) INCH OR GREATER) RAINFALL EVENT AND REPAIR IMMEDIATELY. REMOVE SEDIMENT AND RESTORE THE SEDIMENT STORAGE AREA TO ITS ORIGINAL DIMENSIONS WHEN THE SEDIMENT HAS ACCUMULATED TO ONE-HALF THE DESIGN DEPTH OF THE TRAP. PLACE THE SEDIMENT THAT IS REMOVED IN THE DESIGNATED DISPOSAL AREA AND REPLACE THE CONTAMINATED PART OF THE GRAVEL FACING.
- SEDIMENT BASIN REMOVE SEDIMENT AND RESTORE THE BASIN TO ITS ORIGINAL DIMENSIONS WHEN IT ACCUMULATES TO WITHIN ONE HALF OF THE DESIGN DEPTH. PLACE SEDIMENT IN AN AREA WITH SEDIMENT CONTROLS. CHECK THE EMBANKMENT, SPILLWAYS, AND OUTLET FOR EROSION DAMAGE, PIPING, AND SETTLEMENT. MAKE ALL NECESSARY REPAIRS IMMEDIATELY. REMOVE ALL TRASH AND OTHER DEBRIS FROM THE RISER AND POOL AREA.
- SKIMMER INSPECT SKIMMER AT LEAST ONCE A WEEK AND AFTER EACH RAINFALL TO MAKE SURE THAT THE INTAKE MECHANISM, ORIFICE, OR DISCHARGE PIPE IS NOT CLOGGED WITH TRASH OR SEDIMENT. IF THE BASIN IS DRY, MAKE SURE THAT ANY VEGETATION GROWING ON THE BOTTOM IS NOT HOLDING THE SKIMMER DOWN. TAKE SPECIAL PRECAUTION IN WINTER TO PREVENT THE SKIMMER FROM PLUGGING
- OUTLET PROTECTION INSPECT RIP RAP OUTLET STRUCTURES WEEKLY AND AFTER SIGNIFICANT (\$\frac{1}{2}\) INCH OR GREATER) RAINFALL EVENTS TO SEE IF ANY EROSION AROUND OR BELOW THE RIP RAP HAS TAKEN PLACE, OR IF STONES HAVE BEEN DISLODGED. IMMEDIATELY MAKE ALL NEEDED REPAIRS TO PREVENT FURTHER DAMAGE.
- EMERGENCY SPILLWAY / FOREBAY PROTECTION AFTER EVERY HIGH-WATER EVENT INSPECT THE INTEGRITY OF THE LINED SPILLWAY AND THE ADJACENT EARTHEN BANKS. IMMEDIATELY MAKE ALL NEEDED REPAIRS TO PREVENT FURTHER DAMAGE. REPAIR ANY VOIDS IN THE RIP RAP LINED APRONS, RE-ESTABLISH ANY LOOSE STONES, AND FIX GAPS IN THE ADJACENT VEGETATIVE COVER.

RATE (LB/ACRE)

PERMANENT SEEDING SCHEDULE - MIXTURE 5CP Seeding Mixture

PLAN TO MAINTAIN A VIGOROUS, DENSE VEGETATIVE COVER.

Seeding Dates

SPECIES

CENTIPEDE GRASS

MARCH - JUNE

Soil Amendments APPLY LIME AND FERTILIZER ACCORDING TO SOIL TESTS, OR APPLY 300 LB/ACRE 10-10-10 FERTILIZER.

Mulch DO NOT MULCH.

FERTILIZE VERY SPARINGLY--20 LB/ACRE NITROGEN IN SPRING WITH NO PHOSPHORUS. CENTIPEDE GRASS CANNOT TOLERATE HIGH PH

TEMPORARY SEEDING-LATE WINTER/EARLY SPRING

Seeding Mixture

WINTER AND EARLY SPRING

SPECIES	RATE (LB/ACR
RYE (GRAIN)	120
ANNUAL LESPEDEZA (KOBE IN PIEDMONT AND COASTAL PLAIN, KOREAN IN MOUNTAINS)	50

OMIT ANNUAL LESPEDEZA WHEN DURATION OF TEMPORARY COVER IS NOT TO EXTEND BEYOND JUNE.

SUMMER

SPECIES		RATE (LB/ACRE)
GERMAN MILLET		40
	<u>FALL</u>	
<u>SPECIES</u>		RATE (LB/ACRE)
RYE (GRAIN)		120

Seeding dates

COASTAL PLAIN - DEC.1-APR. 15 - LATE WINTER AND EARLY SPRING APRIL 15 - AUG. 15 - SUMMER AUG. 15 - DEC. 30 - FALL

Soil amendments

FOLLOW RECOMMENDATIONS OF SOIL TESTS OR APPLY 2,000 LB/ACRE GROUND AGRICULTURAL LIMESTONE AND 750 LB/ACRE 10-10-10 FERTILIZER.

APPLY 4,000 LB/ACRE STRAW. ANCHOR STRAW BY TACKING WITH ASPHALT, NETTING OR A MULCH ANCHORING TOOL. A DISK WITH BLADES SET NEARLY STRAIGHT CAN BE USED AS A MULCH ANCHORING TOOL.

Maintenance

REFERTILIZE IF GROWTH IS NOT FULLY ADEQUATE. RESEED, REFERTILIZE AND MULCH IMMEDIATELY FOLLOWING EROSION OR OTHER

SITE NOTES

- ANY DISCREPANCY IN THIS PLAN AND ACTUAL FIELD CONDITIONS SHALL BE REPORTED TO THE OWNER PRIOR TO START OF CONSTRUCTION. GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF ALL SETBACKS, EASEMENTS AND DIMENSIONS SHOWN HEREON BEFORE BEGINNING CONSTRUCTION.
- CONTRACTOR SHALL MAINTAIN THE SITE IN A MANNER SO THAT WORKMEN AND PUBLIC SHALL BE PROTECTED FROM INJURY, AND ADJOINING PROPERTY PROTECTED FROM DAMAGE.
- ACCESS TO UTILITIES, FIRE HYDRANTS, STREET LIGHTING, ETC., SHALL REMAIN UNDISTURBED, UNLESS COORDINATED WITH THE
- 4. DO NOT SCALE THIS DRAWING AS IT IS A REPRODUCTION AND SUBJECT TO DISTORTION.
- THE GENERAL CONTRACTOR SHALL REMOVE ALL DEBRIS FROM THE SITE UPON COMPLETION OF THE PROJECT AND AT LEAST ONCE A WEEK DURING CONSTRUCTION.

DISTURBED OR DAMAGED IN ANY MANNER AS A RESULT OF CONSTRUCTION SHALL BE REPLACED OR REPAIRED IN ACCORDANCE WITH

- 6. THE GENERAL CONTRACTOR SHALL KEEP THE AREA OUTSIDE THE "CONSTRUCTION LIMITS" BROOM CLEAN AT ALL TIMES.
- 7. ALL UTILITIES TO SERVICE PROPOSED STRUCTURE SHALL BE UNDERGROUND ON SITE. 8. ALL STREET SURFACES, DRIVEWAYS, CULVERTS, CURB AND GUTTERS, ROADSIDE DRAINAGE DITCHES AND OTHER STRUCTURES THAT ARE
- CONTRACTOR SHALL MAINTAIN AN "AS-BUILT" SET OF DRAWINGS TO RECORD THE EXACT LOCATION OF ALL PIPING PRIOR TO CONCEALMENT. DRAWINGS SHALL BE GIVEN TO THE OWNER UPON COMPLETION OF THE PROJECT WITH A COPY OF THE TRANSMITTAL
- 10. IF DEPARTURES FROM THE SPECIFICATIONS OR DRAWINGS ARE DEEMED NECESSARY BY THE CONTRACTOR, DETAILS OF SUCH EPARTURES AND REASONS THEREOF SHALL BE GIVEN TO THE OWNER FOR REVIEW, NO DEPARTURES FROM THE CONTRACT DOCUMEN SHALL BE MADE WITHOUT THE PERMISSION OF THE OWNER, THE CITY OF WILMINGTON, NEW HANOVER COUNTY, OR CFPUA,
- CONTRACTOR SHALL VERIFY LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES. THE LOCATION OF ALL EXISTING UTILITIES ARE NOT NECESSARILY SHOWN ON PLANS AND WHERE SHOWN ARE ONLY APPROXIMATE. THE CONTRACTOR SHALL ON HIS INITIATIVE AND AT NO EXTRA COST HAVE LOCATED ALL UNDERGROUND LINES AND STRUCTURES AS NECESSARY. NO CLAIMS FOR DAMAGES OR EXTRA COMPENSATION SHALL ACCRUE TO THE CONTRACTOR FROM THE PRESENCE OF SUCH PIPE OTHER OBSTRUCTIONS OR FROM DELAY DUE TO REMOVAL OR REARRANGEMENT OF THE SAME. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UNDERGROUND STRUCTURES. CONTACT NORTH CAROLINA ONE CALL" TOLL FREE 1-800-632-4949 AT LEAST 48 HOURS PRIOR TO CONSTRUCTION. THE
- 12. THE CONTRACTOR SHALL OBTAIN AND PAY FOR ALL INSPECTIONS, CERTIFICATIONS, EQUIPMENT, ETC., THAT MAY BE REQUIRED.
- THE ENGINEER AND/OR OWNER DISCLAIM ANY ROLE IN THE CONSTRUCTION MEANS AND METHODS ASSOCIATED WITH THE PROJECT AS SET FORTH IN THESE PLANS.
- 14. ALL LOT STRIPING AND DIRECTIONAL ARROWS TO BE REFLECTIVE MARKINGS AND SHALL CONFORM TO NCDOT. ALL PARKING STALL MARKINGS AND LANE ARROWS WITHIN THE PARKING AREAS SHALL BE WHITE.
- 16. LANDSCAPE PLANTINGS AT ENTRANCE/ EXITS WILL BE INSTALLED AND MAINTAINED SO AS NOT TO INTERFERE WITH SIGHT DISTANCE NEEDS OF DRIVERS IN THE PARKING AREA AND AT ENTRANCE/EXIT LOCATIONS PER LOCAL STANDARDS. 17. SEEDING TO BE INSTALLED TO LOCAL REQUIREMENTS & STANDARD PRACTICES.

15. ALL PARKING LOT DIMENSIONS ARE FROM FACE OF CURB UNLESS NOTED OTHERWISE.

CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL NONSUBSCRIBING UTILITIES.

18. ALL DIMENSIONS AND RADII ARE TO OUTSIDE FACE OF BUILDING OR TO FACE OF CURB UNLESS OTHERWISE NOTED

NC ACCESSIBILITY NOTES:

COMPLY WITH THE GUIDELINES FOR RAMPS OR CURB RAMPS.

HANDRAILS WHERE HANDRAILS ARE PROVIDED (NC BUILDING CODE 1104.1 & 1104.2).

A117.1, THE NC BUILDING CODE, AND APPLICABLE LOCAL LAWS & REGULATIONS

CLEAR LANDING OF SIXTY (60) INCHES BY SIXTY (60) INCHES MINIMUM.

- SPECIAL ATTENTION SHALL BE GIVEN TO COMPLIANCE WITH AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS), THE NORTH CAROLINA BUILDING CODE/ANSI A117.1, AND APPLICABLE LOCAL LAWS & REGULATIONS.
- IT IS ESSENTIAL THAT CONTRACTORS ARE AWARE OF THE SITE ACCESSIBILITY REQUIREMENTS. PARAMOUNTE ENGINEERING HAS DEVELOPED THESE NOTES AND DETAILS TO ASSURE THAT CONTRACTORS ARE AWARE OF THE REQUIREMENTS AT THE POINT IN TIME WHEN THEYARE BIDDING THE PROJECT. IN ADDITION, PARAMOUNTE ENGINEERING HAS MADE A POINT IN THESE NOTES AND DETAILS, AS WELL AS IN OUR DRAWINGS, TO PROVIDE SLOPES / GRADES AND DIMENSIONS THAT COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS), THE NORTH CAROLINA BUILDING CODE/ANSI A117.1 AND APPLICABLE LOCAL LAWS & REGULATIONS. IF THESE SLOPES / GRADES AND DIMENSIONS ARE NOT ACHIEVABLE, THE CONTRACTOR IS REQUIRED TO CONTACT THE OWNER IMMEDIATELY AND BEFORE MOVING FORWARD WITH THE WORK.
- THE CONTRACTOR SHALL NOTIFY PARAMOUNTE ENGINEERING IMMEDIATELY OF ANY CONFLICT BETWEEN THESE NOTES AND DETAILS AND OTHER PROJECT DRAWINGS, WHETHER BY PARAMOUNTE ENGINEERING OR OTHERS. THE CONTRACTOR SHALL NOT PROCEED WITH THE WORK FOR WHICH THE ALLEGED CONFLICT HAS BEEN DISCOVERED UNTIL SUCH ALLEGED CONFLICT HAS BEEN RESOLVED. NO CLAIM SHALL BE MADE BY THE CONTRACTOR FOR DELAY OR DAMAGES AS A RESULT OF RESOLUTION OF ANY SUCH CONFLICT(S).
- 4. THESE ACCESSIBILITY NOTES AND DETAILS ARE INTENDED TO DEPICT SLOPE AND DIMENSIONAL REQUIREMENTS ONLY. REFER TO SIDEWALK, CURBING, AND PAVEMENT DETAILS FOR ADDITIONAL INFORMATION.
- AT LEAST ONE ACCESSIBLE ROUTE SHALL BE PROVIDED WITHIN THE SITE FROM ACCESSIBLE PARKING SPACES AND ACCESSIBL ${\tt PASSENGER\ LOADING\ ZONES; PUBLIC\ STREETS\ OR\ SIDEWALKS; AND\ PUBLIC\ TRANSPORTATION\ STOPS\ TO\ THE\ ACCESSIBLE\ BUILDING\ OR\ STOPS\ TO\ THE\ ACCESSIBLE\ THE\ ACCESSIBLE\$
- 2. AT LEAST ONE ACCESSIBLE ROUTE SHALL CONNECT ACCESSIBLE BUILDINGS, ACCESSIBLE FACILITIES, ACCESSIBLE ELEMENTS, AND ACCESSIBLE SPACES THAT ARE ON THE SAME SITE.
- 3. WALKING SURFACES THAT ARE PART OF AN ACCESSIBLE ROUTE SHALL HAVE A MAXIMUM RUNNING SLOPE OF 5.0% AND A MAXIMUM CROSS 4. ANY WALKING SURFACE THAT IS PART OF AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 5.0% IS A RAMP AND SHALL
- TRANSITIONS BETWEEN RAMPS, WALKS, LANDINGS, GUTTERS OR STREETS SHALL BE FLUSHAND FREE OF ABRUPT VERTICAL CHANGES (1/4 INCH MAXIMUM VERTICAL CHANGE IN LEVEL PERMITTED).
- 6. FLOOR SURFACES SHALL BE STABLE, FIRM AND SLIP RESISTANT. 7. THE MINIMUM CLEAR WIDTH OF EXTERIOR ACCESSIBLE ROUTES SHALL BE FORTY-EIGHT (48) INCHES MINIMUM MEASURED BETWEEN
- 8. WHERE AN ACCESSIBLE ROUTE MAKES A 180 DEGREE TURN AROUND AN OBJECT THAT IS LESS THAN FORTY-EIGHT (48) INCHES IN WIDTH CLEAR WIDTH SHALL BE FORTY-TWO (42) INCHES MINIMUM APPROACHING THE TURN, FORTY-EIGHT (48) INCHES MINIMUM DURING THE TURN, AND FORTY-TWO (42) INCHES MINIMUM LEAVING THE TURN. THE CLEAR WIDTH APPROACHING AND LEAVING THE TURN MAY BE THIRTY-SIX (36) INCHES MINIMUM WHEN THE CLEAR WIDTH AT THE TURN IS SIXTY (60) INCHES MINIMUM. .* SEE NOTE 7 ABOVE FOR NC CLEAR WIDTH OF EXTERIOR ACCESSIBLE ROLITES*
-). AN ACCESSIBLE ROUTE WITH A CLEAR WIDTH LESS THAN SIXTY (60) INCHES SHALLPROVIDE PASSING SPACES AT INTERVALS OF TWO HUNDRED (200) FEET MAXIMUM. PASSING SPACES SHALL BE EITHER A SIXTY (60) INCH MINIMUM BY SIXTY (60) INCH MINIMUM SPACE; OR AN INTERSECTION OF TWO (2) WALKING SURFACES THAT PROVIDE A COMPLIANT T-SHAPED TURNING SPACE, PROVIDED THE BASE AND ARMS OF THE T-SHAPED SPACE EXTEND FORTY-EIGHT (48) INCHES MINIMUM BEYOND THE INTERSECTION.
- 10. DOORS, DOORWAYS AND GATES THAT ARE PART OF AN ACCESSIBLE ROUTE SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS), THE NORTH CAROLINA BUILDING CODE/ ANSI A117.1, AND APPLICABLE LOCAL LAWS & REGULATIONS.
- 11. DIRECTIONAL SIGNAGE INDICATING THE ROUTE TO THE NEAREST ACCESSIBLE BUILDING ENTRANCE SHALL BE PROVIDED AT INACCESSIBLE 12. WHERE POSSIBLE, DRAINAGE INLETS SHALL NOT BE LOCATED ON AN ACCESSIBLE ROUTE. IN THE EVENT THAT A DRAINAGE INLET MUST BE LOCATED ON AN ACCESSIBLE ROUTE, THE GRATE SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS),
- 1. ANY PART OF AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 5% SHALL BE CONSIDERED A RAMP.
- 2. THE MAXIMUM RUNNING SLOPE FOR A RAMP SHALL BE 8.33% AND THE MAXIMUM CROSS SLOPE SHALL BE 2.0%.
- 3. THE CLEAR WIDTH OF AN EXTERIOR RAMP RUN SHALL BE FORTY EIGHT INCHES (NC BUILDING CODE 1104.1). WHERE HANDRAILS ARE PROVIDED ON THE RAMP RUN, THE CLEAR WIDTH SHALL BE MEASURED BETWEEN THE HANDRAILS.
- 4. THE RISE FOR ANY RAMP RUN SHALL BE THIRTY (30) INCHES MAXIMUM. 5. LANDINGS SHALL BE PROVIDED AT THE TOP AND BOTTOM OF RAMPS. LANDINGS SHALL HAVE A SLOPE NOT STEEPER THAN 2.0% IN ANY DIRECTION. THE LANDING CLEAR WIDTH SHALL BE AT LEAST AS WIDE AS THE WIDEST RAMP RUN LEADING TO THE LANDING. THE LANDING CLEAR LENGTH SHALL BE SIXTY (60) INCHES LONG MINIMUM. RAMPS THAT CHANGE DIRECTION BETWEEN RUNS AT LANDINGS SHALL HAVE A
- 6. RAMP RUNS WITH A RISE GREATER THAN SIX (6) INCHES SHALL HAVE HANDRAILS ON BOTH SIDES COMPLYING WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS), THE NC BUILDING CODE/ANSI A117.1, AND APPLICABLE LOCAL LAWS & REGULATIONS.
- 7. FLOOR SURFACES OF RAMPS AND LANDINGS SHALL BE STABLE, FIRM AND SLIP RESISTANT.
- APPLICABLE LOCAL LAWS & REGULATIONS SHALL BE PROVIDED ON EACH SIDE OF RAMP RUNS AND ON EACH SIDE OF RAMP LANDINGS. 9. WHERE DOORWAYS ARE LOCATED ADJACENT TO A RAMP LANDING, MANEUVERING CLEARANCES REQUIRED BY THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS). THE NC BUILDING CODE/ANSI A117.1 SHALL BE PERMITTED TO OVERLAP THE REQUIRED LANDING AREA. WHERE DOORS THAT ARE SUBJECT TO LOCKING ARE ADJACENT TO A RAMP LANDING, LANDINGS SHALL BE SIZED TO PROVIDE A COMPLIANT TURNING SPACE.

CURB RAMP NOTES:

- 1. THE MAXIMUM RUNNING SLOPE OF A CURB RAMP SHALL BE 8.33% AND THE MAXIMUM CROSS SLOPE SHALL BE 2.0%. 2. COUNTER SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE CURB RAMP SHALL NOT BE STEEPER THAN 5%. THE ADJACENT SURFACES AT TRANSITIONS AT CURB RAMPS TO WALKS, GUTTERS AND STREETS SHALL BE AT THE SAME LEVEL.
- 3. THE CLEAR WIDTH OF A CURB RAMP SHALL BE 36 INCHES (36) MINIMUM, EXCLUSIVE OF FLARED SIDES, IF PROVIDED. *NOTE NC BUILDING CODE REQUIRES EXTERIOR ACCESSIBLE ROUTES TO BE 48 INCHES MINIMUM WIDE (1104.1 & 1104.2). 4. LANDINGS SHALL BE PROVIDED AT THE TOP OF CURB RAMPS. THE CLEAR LENGTH OF THE LANDING SHALL BE THIRTY-SIX (36) INCHES MINIMUM. THE CLEAR WIDTH OF THE LANDING SHALL BE AT LEAST AS WIDE AS THE CURB RAMP, EXCLUDING FLARED SIDES, LEADING TO THE LANDING. LANDINGS SHALL HAVE A SLOPE NOT STEEPER THAN 2% IN ANY DIRECTION.
- 5. IF A CURB RAMP IS LOCATED WHERE PEDESTRIANS MUST WALK ACROSS THE RAMP, OR WHERE IT IS NOT PROTECTED BY HANDRAILS OR GUARDRAILS, IT SHALL HAVE FLARED SIDES.
- 7. CURB RAMPS AND THE FLARED SIDES OF CURB RAMPS SHALL BE LOCATED SO THAT THEY DO NOT PROJECT INTO VEHICULAR TRAFFIC LANES, PARKING SPACES OR PARKING ACCESS AISLES. CURBS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE
- 8. CURB RAMPS SHALL BE LOCATED OR PROTECTED TO PREVENT THEIR OBSTRUCTION BY PARKED VEHICLES. 9. IT IS RECOMMENDED TO PROVIDE CURB RAMPS WITH A TWENTY-FOUR (24) INCH DEEP DETECTABLE WARNING COMPLYING WITH 406.12 A117.1, EXTENDING THE FULL WIDTH OF THE RAMP. REFERTO DETECTABLE WARNING DETAILS AND NOTES FOR PLACEMENT, ORIENTATION
- AND NOTES. THE NC BUILDING CODE DOES NOT CURRENTLY REQUIRE DETECTABLE WARNINGS AT CURB RAMPS, NOR DO THE 2010 ADA STANDARDS - HOWEVER US DOT ADA REGULATIONS DO REQUIRE THESE.
- 10. FLOOR SURFACES OF CURB RAMPS SHALL BE DEEP GROOVED, ½ INCH WIDE BY ¼ INCH DEEP, ONE (1) INCH CENTERS TRANSVERSE TO THE

6. WHERE PROVIDED, CURB RAMP FLARES SHALL NOT EXCEED 10%.

- 11. WHERE PROVIDED, STOP LINES SHALL BE LOCATED IN ADVANCE OF CURB RAMP. 12. WHERE PROVIDED, PEDESTRIAN ACTIVATED SIGNALS SHALL BE LOCATED ADJACENT TO THE SIDEWALK AND NOT ON THE SIDEWALK. 13. WHERE PROVIDED, DRAINAGE INLETS SHALL BE LOCATED UPSTREAM OF CURB RAMPS AND NOT IN THE RAMP AREA.
- 14. CURB RAMP TYPE AND LOCATION ARE PER PLAN. PARKING SPACE NOTES:
- . ACCESSIBLE PARKING SPACES SHALL BE LOCATED ON THE SHORTEST ACCESSIBLE ROUTES OF TRAVEL FROM ADJACENT PARKING TO AN ACCESSIBLE BUILDING ENTRANCE. 2. ACCESSIBLE PARKING SPACES SHALL BE AT LEAST NINETY-SIX (96) INCHES WIDE. ACCESS AISLES SHALL BE 60 INCHES WIDE. ONE OF SIX ACCESSIBLE SPACES SHOULD PROVIDE A VAN ACCESSIBLE AISLE. THE AISLE SHOULD BE 96 INCHES WIDE (OR ACCESSIBLE SPACE IS 11 FEET AND ACCESS AISLE IS FIVE FEET). WHERE PARKING SPACES AND ACCESS AISLES ARE MARKED WITH LINES, THE WIDTH MEASUREMENTS SHALL BE MADE FROM CENTERLINE OF THE MARKINGS. WHERE PARKING SPACES OR ACCESS AISLES ARE NOT ADJACENT TO ANOTHER
- PARKING SPACE OR ACCESS AISLE. 3. PARKING ACCESS AISLES SHALL BE PART OF AN ACCESSIBLE ROUTE TO THE BUILDING OR FACILITY ENTRANCE AND SHALL COMPLY WITH PROVISIONS FORACCESSIBLE ROUTES. MARKED CROSSINGS SHALL BE PROVIDED WHERE THE ACCESSIBLE ROUTE MUST CROSS VEHICULAR

PARKING SPACE OR ACCESS AISLES, MEASUREMENTS SHALL BE PERMITTED TO INCLUDE THE FULL WIDTH OF THE LINE DEFINING THE

- TRAFFIC LANES. WHERE POSSIBLE, IT IS PREFERABLE THAT THE ACCESSIBLE ROUTE NOT PASS BEHIND PARKED VEHICLES.
- 4. TWO (2) ACCESSIBLE PARKING SPACES MAY SHARE A COMMON ACCESS AISLE.

CLEARANCES AND THE LOCATION OF VAN ACCESSIBLE PARKING SPACES.

ANOTHER COLOR THAT CAN BE DISTINGUISHED FROM PAVEMENT).

PASSENGER LOADING ZONE NOTES:

HUNDRED FOURTEEN (114) INCHES MINIMUM.

ACCESSIBLE ENTRANCE NOTES:

- 5. ACCESS AISLES SHALL EXTEND THE FULL LENGTH OF THE PARKING SPACE THEY SERVE.
- 6. ACCESS AISLES SHALL BE MARKED SO AS TO DISCOURAGE PARKING IN THEM. 7. ACCESS AISLES SHALL NOT OVERLAP THE VEHICULAR WAY. ACCESS AISLES SHALL BE PERMITTED TO BE PLACED ON EITHER SIDE OF THE PARKING SPACE EXCEPTFOR ANGLED VAN PARKING SPACES WHICH SHALL HAVE ACCESS AISLES LOCATED ON THE PASSENGER SIDE OF THE
- 8. FLOOR SURFACES OF PARKING SPACES AND ACCESS AISLES SERVING THEM SHALL BE STABLE, FIRM AND SLIP RESISTANT. ACCESS AISLES SHALL BE AT THE SAME LEVEL AS THE PARKING SPACES THEY SERVE. CHANGES IN LEVEL ARE NOT PERMITTED.
- 10. PARKED VEHICLE OVERHANGS SHALL NOT REDUCE THE REQUIRED CLEAR WIDTH OF AN ACCESSIBLE ROUTE. 11. PARKING SPACES FOR VANS AND ACCESS AISLES AND VEHICULAR ROUTES SERVING THEM SHALL PROVIDE A VERTICAL CLEARANCE OF NINETY-EIGHT (98) INCHES MINIMUM. SIGNS SHALL BE PROVIDED AT ENTRANCES TO PARKING FACILITIES INFORMING DRIVERS OF

9. PARKING SPACES AND ACCESS AISLES SHALL BE LEVEL WITH SURFACE SLOPES NOT EXCEEDING 2.0% IN ALL DIRECTIONS.

- 2. EACH ACCESSIBLE PARKING SPACE SHALL BE PROVIDED WITH SIGNAGE DISPLAYING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY. SIGNS SHALL BE INSTALLED AT A MINIMUM CLEAR HEIGHT OF SIXTY (60) INCHES ABOVE GRADE AND SHALL NOT INTERFERE WITH AN ACCESSIBLE ROUTE FROM AN ACCESS AISLE. SIGNS LOCATED WHERE THEY MAY BE HIT BY VEHICLES BEING PARKED SHALL BE INSTALLED WITH BOLLARD
- 3. SIGNAGE AT ACCESSIBLE PARKING SPACES REQUIRED BY THE NC BUILDING CODE SECTION 1106.1SHALL COMPLY WITH THE REQUIREMENTS OF NORTH CAROLINA GENERAL STATUTE 20-37.6 AND 136-30 AND THE NCDOT UNIFORM MANUAL ON TRAFFIC CONTROL DEVICES. A SEPARATE SIGN IS REQUIRED FOR EACH SPACE. SIGNS TO INDICATE THE MAXIMUM PENALTY MUST BE PROVIDED AT EACH ACCESSIBLE SPACE. 14. ACCESSIBLE PARKING SPACE, ACCESS AISLE STRIPING, AND INTERNATIONAL SYMBOL OF ACCESSIBILITY SHALL BE PAINTED BLUE (OR
- 1. PASSENGER LOADING ZONES SHALL PROVIDE VEHICULAR PULL-UP SPACE NINETY-SIX (96) INCHES WIDE MINIMUM AND TWENTY (20) FEET LONG PASSENGER LOADING ZONES SHALL PROVIDE A CLEARLY MARKED ACCESS AISLE THAT IS SIXTY (60) INCHES WIDE MINIMUM AND EXTENDS
- THE FULL LENGTH OF THE VEHICLE PULL-UP SPACE THEY SERVE. 3. ACCESS AISLE SHALL ADJOIN AN ACCESSIBLE ROUTE AND NOT OVERLAP THE VEHICULAR WAY.
- 5. FLOOR SURFACES OF VEHICLE PULL-UP SPACES AND ACCESS AISLES SERVING THEM SHALL BE STABLE, FIRM AND SLIP RESISTANT. 6. VEHICLE PULL-UP SPACES, ACCESS AISLES SERVING THEM AND A VEHICULAR ROUTE FROM AN ENTRANCE TO THE PASSENGER LOADING

DIRECTIONS. ACCESS AISLES SHALL BE AT THE SAME LEVEL AS THE VEHICLE PULL-UP SPACE THEY SERVE. CHANGES IN LEVEL ARE NOT

4. VEHICLE PULL-UP SPACES AND ACCESS AISLES SERVING THEM SHALL BE LEVEL WITH SURFACE SLOPES NOT EXCEEDING 2.0% IN ALL

1. ACCESSIBLE ENTRANCES SHALL BE PROVIDED AS REQUIRED BY THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS) AND THE NORTH CAROLINA BUILDING CODE, AND APPLICABLE LOCAL LAWS & REGULATIONS.

ZONE, AND FROM THE PASSENGER LOADING ZONE TO A VEHICULAR EXIT SERVING THEM, SHALL PROVIDE A VERTICAL CLEARANCE OF ONE

2. ENTRANCE DOORS, DOORWAYS AND GATES SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS) THE NC BUILDING CODE/ANSI A117.1 AND SHALL BE ON AN ACCESSIBLE ROUTE.

GENERAL UTILITY NOTES

1. ALL STORM SEWERS SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF WILMINGTON AND NCDOT

REQUIREMENTS AS SPECIFIED ON THE DRAWINGS AND IN THE PROJECT SPECIFICATIONS.

- 2. BEDDING FOR ALL STORM SEWER PIPE SHALL BE AS SPECIFIED ON THE DRAWINGS.
- 3. ALL STORM SEWER PIPES SHOWN AS RCP ON THE PLANS SHALL BE REINFORCED CONCRETE PIPE CONFORMING TO ASTM C-76, UNLESS INDICATED OTHERWISE ON

STORM WATER NOTES

- 1. TOTAL SITE AREA = 654,480 SF (15.03 AC)
- 2. DISTURBED AREA = 609,840 SF (14.00 AC)
- 3. IMPERVIOUS AREA DRAINING TO STORMWATER SYSTEM = 440,000 SF (10.10 AC)

WETLAND NOTES:

ROOF DRAIN NOTE:

1) NO WETLANDS EXIST ON-SITE BASED ON A SITE EVALUATION BY ECS CAROLINAS, LLP.

1) PROPOSED BUILDING SHALL DIVERT ROOF DRAINAGE TO STORMWATER COLLECTION

EXISTING UTILITY NOTES: 1. IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO VERIFY THE ACTUAL LOCATION AND

AVAILABILITY OF ALL EXISTING AND PROPOSED UTILITIES IN THE FIELD PRIOR TO GROUND BREAKING.

2. EXISTING UTILITIES AND STRUCTURES SHOWN, BOTH UNDERGROUND AND ABOVE GROUND, ARE BASED ON A FIELD SURVEY AND THE BEST AVAILABLE RECORD DRAWINGS. THE CONTRACTOR SHALL FIELD VERIFY FIELD CONDITIONS PRIOR TO BEGINNING RELATED CONSTRUCTION. ANY DISCREPANCIES SHALL BE REPORTED TO THE OWNER'S REPRESENTATIVE IMMEDIATELY

 $\sim \sim$

SW=25.05-



= TRANSFORMER PEDESTAL (PAD) = PROPOSED DROP INLET = PROPOSED CURB INLET

= UNDERGROUND TELEPHONE

= PROPOSED DRAINAGE FLOW ARROW

= PROPOSED TREE PROTECTION FENCING

= PROPOSED SANITARY SEWER SERVICE

= PROPOSED CONCRETE CURB & GUTTE

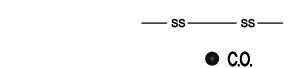
= PROPOSED STORM SEWER = PROPOSED STORM DRAIN MANHOLE (CB-202) = PROPOSED DRAINAGE INLET

EG=25.05 — **= EXISTING SPOT ELEVATION** EP=25.05 — = PROPOSED EDGE OF PAVEMENT TW=25.05 = PROPOSED SIDEWALK ELEVATION

= PROPOSED GUTTER FLOW LINE TC=25.05 — = PROPOSED TOP OF CURB ELEVATION = PROPOSED CONTOUR _____35_____

= PROPOSED DRAINAGE AREAS = PROPOSED SILT FENCE = PROPOSED LIMITS OF DISTURBANCE _____ LOD _____

= PROPOSED INLET PROTECTION = PROPOSED TRAFFIC ARROW



= PROPOSED SANITARY SEWER CLEANOUT

= PROPOSED BUILDING

= PROPOSED SIGN

NPDES WATER QUALITY STABILIZATION TIME FRAMES SITE AREA DESCRIPTION STABILIZATION TIMEFRAME EXCEPTIONS

7 DAYS

14 DAYS

Public Services • Engineering Division APPROVED STORMWATER MANAGEMENT PLAN

Approved Construction Plan

PERIMETER DIKES, SWALES, DITCHES AND SLOPES

ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:

HIGH QUALITY WATER (HQW) ZONES

SLOPES STEEPER THAN 3:1

For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance.

SEWER SHED # AND PLANT:

NONE

Call before you dig. NCDENR PWSS WATER PERMIT #: WATER CAPACITY: DWQ SEWER PERMIT #: SEWER CAPACITY:

SEWER TO FLOW THROUGH NEI: YES or NO (CIRCLE ONE)

IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE

NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED

7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH

NONE. EXCEPT FOR PERIMETERS AND HQW ZONES

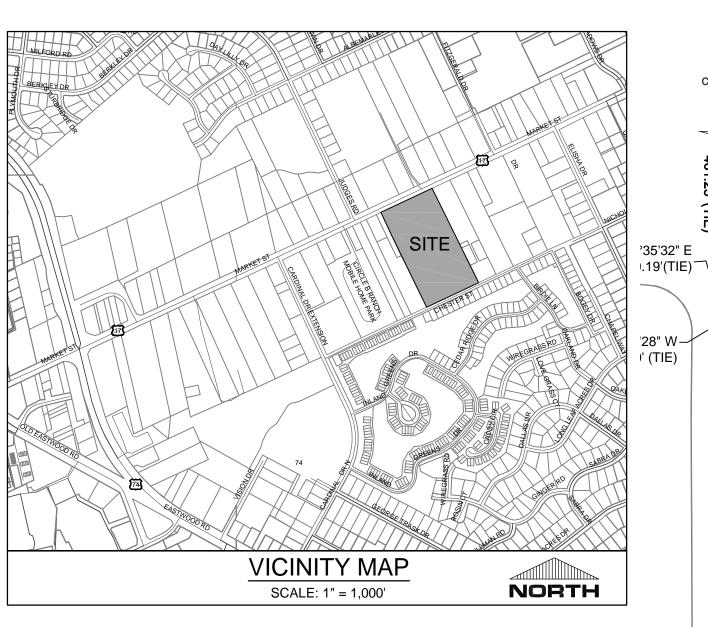
HECKED BY: REV# DATE | DESCRIPTION BY 09 MAR 17 REVISION #1

20-15081.02

GENERAL NOTES

PRELIMINARY DESIGN - NOT RELEASED FOR CONSTRUCTION

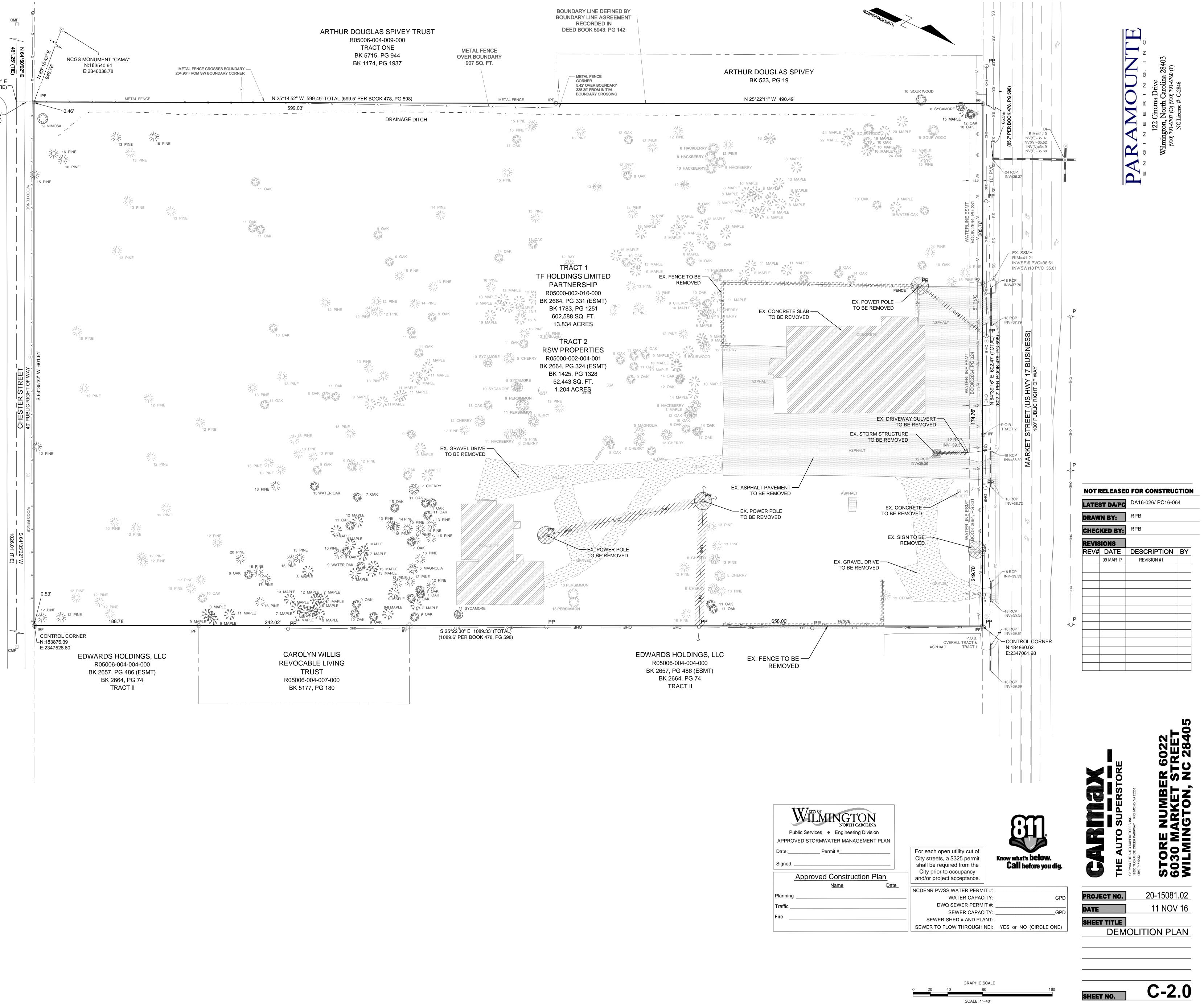
NOT RELEASED FOR CONSTRUCTION

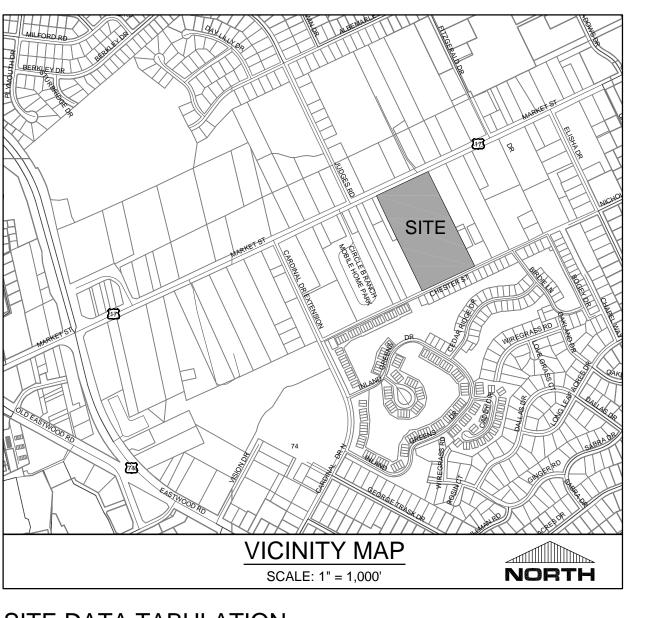


<u>DEMOLITION NOTES:</u> 1.) CONTRACTOR SHALL REFER TO SHEET C-1.0 GENERAL NOTES FOR DEMOLITION NOTES.

- 2.) CONTRACTOR SHALL REFER TO THE LANDSCAPE PLANS FOR SPECIFIC TREE REMOVAL.
- 3.) CONTRACTOR SHALL MAKE NO PAVEMENT CUTS INTO MARKET STREET WITHOUT THE WRITTEN PERMISSION OF NCDOT.
- 4.) ALL UTILITIES SHALL BE ABANDONED AND/OR DEMOLISHED IN ACCORDANCE WITH THE UTILITY OWNERS RULES AND REGULATIONS.+
- 5.) CONTRACTOR SHALL COORDINATE WITH ELECTRIC UTILITY OWNER/PROVIDER FOR DEMOLITION.

SYM	IBOLS LEGEND
	O SURVEY FOR EXISTING TON SYMBOLS LEGEND
	EXISTING ASPHALT TO BE REMOVED
*******	EXISTING CONC. CURB TO BE REMOVED
	EXISTING CONCRETE TO BE REMOVED
	EXISTING GRAVEL TO BE REMOVED
11/1/////	EXISTING UTILITY LINE TO BE REMOVED
	EXISTING STORM SEWER LINE TO BE REMOVED
	EXISTING STORM STRUCTURE TO BE REMOVED
	EXISTING SIGN/LIGHT/UTILITY POLE TO BE REMOVED
<u></u>	EXISTING FENCE TO BE REMOVED
	TREE PROTECTION FENCING





CARMAX OF WILMINGTON

WILMINGTON, NC 28403

BK 1783, PG 1251 - LOT 1

BK 1425, PG 1328 - LOT 2

15.038 ACRES (655,055 SF)

RB - REGIONAL BUSINESS

URBAN

UNDEVELOPED

PROVIDED

141.3 FT. (NORTH)

661 FT. (SOUTH)

254.6 FT. (EAST)

97 FT. (WEST)

6.29%

41,254 SF

384,900 SF

4,350 SF

4,900 SF

4,596 SF

41,254 SF

5.93% (PROPOSED)

28'-0"

R05000-002-010-000 (13.83 AC) - LOT 1

R05000-002-004-001 (1.20 AC) - LOT 2

BK 2664, PG 331 (ESMT) - LOT 1

BK 2664, PG 324 (ESMT) - LOT 2

AUTOMOBILE DEALER - USED

25 FT.

0 FT.

0 FT.

100' MIN.

40% MAX.

15 FT.

654,480 SF (15.03 AC) 43,560 SF (1 AC)

6030 MARKET STREET

SITE DATA TABULATION

PROJECT ADDRESS:

TAX PARCEL IDENTIFICATION #: RECORDED DEED BOOK:

TOTAL SITE AREA: CURRENT ZONING: CAMA LAND USE CLASSIFICATION: MAXIMUM ALLOWABLE BUILDING HEIGHT: EXISTING LAND USE:

PROPOSED LAND USE: SETBACK STANDARDS

MINIMUM FRONT SETBACK MINIMUM SIDE SETBACK MINIMUM SIDE SETBACK MINIMUM REAR SETBACK

SITE INFORMATION LOT AREA LOT WIDTH LOT COVERAGE

PROPOSED IMPERVIOUS AREA TOTAL BUILDING (PROPOSED FOOTPRINT) **ON-SITE PARKING & DRIVEWAYS GRAVEL DRIVE ON-SITE SIDEWALKS FUTURE IMPERVIOUS**

TOTAL PROPOSED IMP. AREA 440,000 SF (10.10 AC) 440,000 / 654,480 SF = 67.2% PROPOSED PERCENT IMPERVIOUS

CONSTRUCTION TYPE: NUMBER OF PROPOSED BUILDINGS PROPOSED BUILDING SF

BUILDING INFORMATION

LOT COVERAGE SITE PARKING

OVERALL HEIGHT

MINIMUM PARKING REQUIRED: (1 PER 500 SF GFA) MAXIMUM PARKING ALLOWABLE:

125 SPACES 215 CUSTOMER PARKING SPACES TOTAL PARKING PROVIDED: 224 SALES DISPLAY AREA SPACES

9 SPACES HANDICAP PARKING REQUIRED: HANDICAP PARKING PROVIDED: 9 SPACES 15 SPACES **BICYCLE PARKING REQUIRED:** BICYCLE PARKING PROVIDED: 15 SPACES

FLOOD NOTE THE SUBJECT PARCEL IS NOT LOCATED WITHIN A 100-YEAR FLOOD ZONE AS INDICATED BY FEMA FLOOD ZONE PANEL 3720314800J BEARING AN EFFECTIVE DATE OF APRIL 3, 2006.

ALL PAVEMENR MARKINGS IN PUBLIC RIGHTS-OF-WAY AND FOR DRIVEWAYS ARE TO

BE THERMOPLASTIC AND MEET CITY AND/ OR NCDOT STANDARDS. ALL SIGNS AND PAVEMENT MARKINGS IN AREAS OPEN TO PUBLIC TRAFFIC ARE TO ALL TRAFFICE CONTROL SIGNS AND MARKINGS OFF THE RIGHT OF WAY ARE TO BE

MAINTAINGED BY THE PROPERTY OWNER IN ACCRODANCE WITH MUTCD ALL PARKING STALL MARKINGS AND LANE ARROWS WITHIN THE PARKING AREAS SHALL BE WHITE.

ANY BROKEN OR MISSING SIDEWALK PANELS, DRIVEWAY PANELS AND CURBING WILL BE REPLACED

VARIANCE REQUESTS 1.) DRIVE ISLE WIDTH

WE WOULD LIKE TO REQUEST A VARIANCE FOR THE SALES DISPLAY AREA BE APPROVED WITH 20' DRIVE AREA AND NOT THE REQUIRED 24'. THE GENERAL PUBLIC IS NOT ALLOWED TO DRIVE IN THE SALES ISLE, ONLY CARMAX EMPLOYEES, AND IT MEETS THE MINIMUM WIDTH FOR FIRE SERVICES. ALL OTHER DRIVE ISLES OUTSIDE THE SALES AREA WILL MEET THE 24' MINIMUM.

2.) EXCEEDING MAXIMUM PARKING

WE WOULD LIKE TO REQUEST A VARIANCE FOR EXCEEDING THE MAXIMUM PARKING SPACES. THE PLAN PROPOSES 215 PARKING SPACES FOR CUSTOMER/EMPLOYEE PARKING. THE MAXIMUM PER CODE IS 125 SPACES. THE PLAN ALSO HAS 224 DISPLAY SPACES IN THE SALES AREA, WHICH ARE SECURED AND GATED WITH NO ACCESS ALLOWED BY THE GENERAL PUBLIC. THE ADDITIONAL PARKING SPACES FOR CUSTOMER/EMPLOYEE PARKING AREA IS FOR THE MONTHLY WHOLESALE AUCTION THAT IS HELD FOR SELLING VEHICLES THAT HAVE BEEN TRADED AND NOT UP TO CARMAX'S STANDARDS.

3.) PRIVATE DRIVEWAYS WE WOULD LIKE TO REQUEST A VARIANCE FOR EXCEEDING THE 500' ALLOWED FOR A PRIVATE DRIVEWAY ON THE BASIS THAT THE PROPOSED DRIVEWAY AND PARKING CONFIGURATION DOES NOT PROVIDE UNSAFE PARKING, MANEUVERING OR ACCESS FOR THE GENERAL PUBLIC

REFER TO DETAIL C-6.0

B HEAVY DUTY ASPHALT PAVING: REFER TO DETAIL C-6.0 (3) MOUNTABLE CONCRETE CURB & GUTTER:

NCGS MONUMENT "CAMA"

N:183540.64

E:2346038.78

REFER TO DETAIL C-6.0 (4) CONCRETE SIDEWALK: REFER TO DETAIL C-6.0

0.53'

CONTROL CORNER

-N:183876.39

E:2347528.80

188.78'

EDWARDS HOLDINGS, LLC

R05006-004-004-000

BK 2657, PG 486 (ESMT)

BK 2664, PG 74

TRACT II

ZONED RB

(5) CONCRETE DOOR LANDING: NOT APPLICABLE

(1) CONCRETE PAVING:

REFER TO DETAIL C-6.0

(A) LIGHT DUTY ASPHALT PAVING:

REFER TO DETAIL C-6.0

REFER TO DETAIL C-6.0 (7) HANDICAP PARKING SIGN: REFER TO DETAIL C-6.0

(6) STEEL BOLLARD:

8 CONCRETE SIDEWALK: PER CITY/STATE DESIGN STANDARDS. IF NO STANDARDS, REFER TO DETAIL C-6.0

9 HANDICAP PARKING SYMBOL:

(10) HANDICAP ACCESS UNLOADING ZONE: SLOPE 2% MAX. EACH WAY (ADA COMPLIANT) AND STRIPE AS SHOWN.

SCREEN WALL: REFER TO DETAIL C-6.2

SCREEN WALL GATES: REFER TO DETAIL C-6.2

(13) CONCRETE BUMPER BLOCK: 8"w x 5"h x 6'-0 LONG CONCRETE. ANCHOR TO PAVING WITH (2) 1'-6" LONG #4 REBAR. REFER TO DETAIL C-6.0

PARKING LOT LIGHTING: REFER TO SITE LIGHTING PLAN FOR LOCATION AND TYPE.

(15) LIMITS OF NEW PAVING:

N NEW PARKING AND DRIVE AREA NOT DESIGNATED AS HEAVY DUTY (16) STANDARD 24" CURB & GUTTER: REFER TO DETAIL C-6.0

BICYCLE RACK (5 BIKE MIN. PER RACK): CONTRACTOR SHALL SUBMIT SHOP DWG. FOR

PROVIDE 4" WIDE PARKING LOT STRIPING AS SHOWN. USE HIGHWAY MARKING PAINT - WHITE (2 COATS).

DETECTIBLE WARNING MAT: REFER TO DETAIL C-6.0

SIGN LOCATION:
SIGN FURNISHED AND INSTALLED BY OWNER.

REFER TO ARCHITECTURAL PLANS FOR LOCATION (21) CONCRETE DRIVE APRON: NOT APPLICABLE

METAL GUARD RAIL: REFER TO DETAIL C-6.0 23 CONCRETE FLUME: NOT APPLICABLE

RETAINING WALL: NOT APPLICABLE

DESIGNATED ACCESSIBLE ROUTE: PROVIDE STRIPING AND SLOPE 5% MAXIMUM IN DIRECTION OF TRAVEL WITH 2% MAXIMUM CROSS SLOPE.

(26) ORNAMENTAL FENCING: REFER TO DETAIL C-6.0

ARTHUR DOUGLAS SPIVEY TRUST R05006-004-009-000 TRACT ONE

BK 5715, PG 944

BK 1174, PG 1937

ZONED RB

CARWASH/FQC

3,708 SF

FFE=43.00

_ _ _ _ _ _ _ _

PROPOSED · MAN GATE

PROPOSED FUEL TANK AND — (

ARCHITECTURAL PLANS

S 25°22'30" E 1089.33' (TOTAL)

(1089.6' PER BOOK 478, PG 598)

DISPENSER. SEE

FOR DETAILS

N 25°14'52" W 599.49'-TOTAL (599.5' PER BOOK 478, PG 598)

CAROLYN WILLIS

REVOCABLE LIVING

TRUST

R05006-004-007-000

BK 5177, PG 180

ZONED RB

PROPOSED 12'

ACCESS GATE

WIDE, 4' CHAIN LINK

PROPOSED 12' WIDE GRAVEL -ACCESS DRIVE FOR STORM

WATER BASIN MAINTENANCE

27) 4' CHAINLINK FENCE (BLACK): SEE DETAIL C-X.X

6' CHAINLINK FENCE (BLACK) W/ VISION SLATS: SEE DETAIL C-X.X

29 CONCRETE SIDEWALK DETAIL: 6.5" EXPOSED FACE TO ASPHALT SEE DETAIL C-6.0

30 EMERGENCY VEHICLE LANE MARKING: NOT APPLICABLE

HEADER CURB: SEE DETAIL C-6.0 32 TREE PROTECTION FENCING: SEE TREE REMOVAL PLAN C-2.3

CLASS A RIP-RAP STONE: IN ACCORDANCE WITH NCDOT STANDARD SPECIFICATIONS MANUAL, SECTION 1043.

PP

34 SITE LIGHTING: SEE SITE PHOTOMETRIC PLANS FOR POLE AND FOUNDATION DESIGN.

A SERVICE

EDWARDS HOLDINGS, LLC

R05006-004-004-000

BK 2657, PG 486 (ESMT)

BK 2664, PG 74

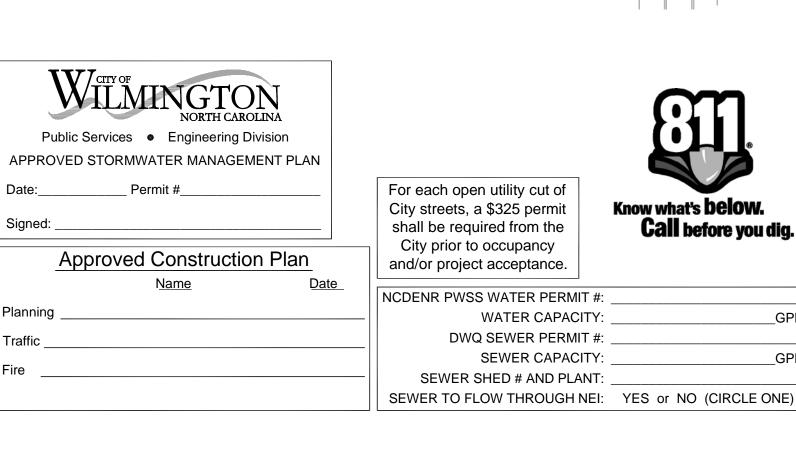
TRACT II

ZONED RB

1,330 SF

2,430 SF

REFER TO DETAIL C-6.9



Know what's **below. Call** before you dig.

PROJECT NO. SHEET TITLE

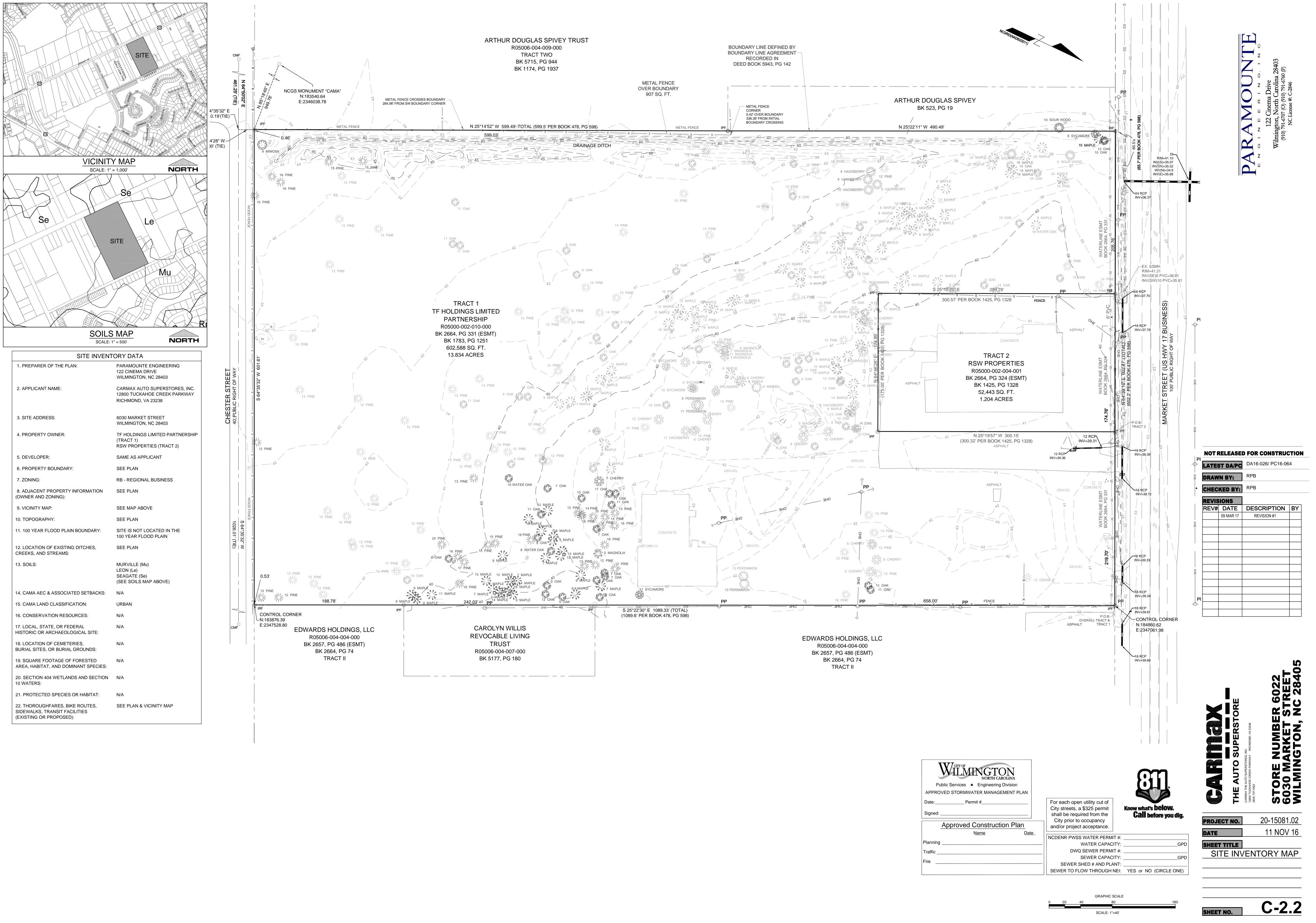
PRELIMINARY DESIGN - NOT RELEASED FOR CONSTRUCTION

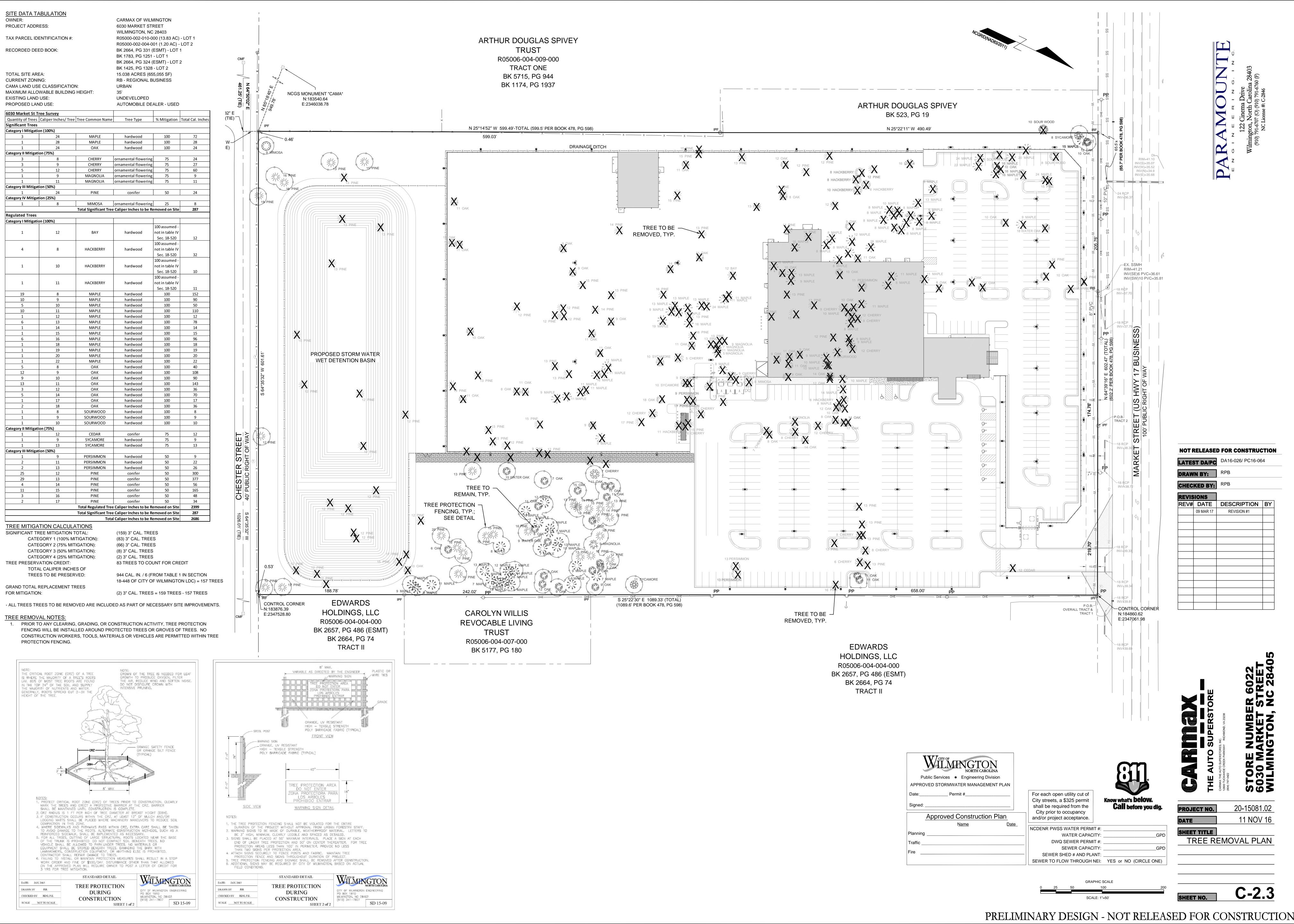
GRAPHIC SCALE

ARTHUR DOUGLAS SPIVEY BK 523, PG 19 **ZONED RB** 10 SOUR WOOD N 25°22'11" W 490.49' 8 SYCAMORE WY INV(S)=35.0 INV(N)=34.9 🚆 PROPOSED — MAN GATE |-17'-|-20'--|-3|| --|-20'--|-3|| EX. SSMH RIM=41.21 INV(SE)6 PVC=36.61 INV(SW)10 PVC=35.81 |--17'--|--17'--|--20'---|--17'-SALES 5,053 SF W/ CANOPY |--17'--|--20'--|--17'- **NOT RELEASED FOR CONSTRUCTION** LATEST DA/PC DRAWN BY: CHECKED BY: RPB REV# DATE DESCRIPTION BY 09 MAR 17 **REVISION #1** 24'--|--20'--|--20'--|--24'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|--20'--|-658,00' PROPOSED CITY OF WILMINGTON — CONTROL CORNER STANDARD COMMERCIAL DRIVEWAY. SEE DETAIL N:184860.62 ± E:2347061.98 20' X 70' SIGHT TRIANGLE, TYP.

689 20-15081.02 11 NOV 16 SITE PLAN

C-2.1





CUT/FILL OPERATIONS TO PROVIDE A COMPACTED CONTROLLED SUBGRADE, IN ACCORDANCE WITH THE SUBSURFACE GEOTECHNICAL EXPLORATION AND TECHNICAL SPECIFICATIONS.

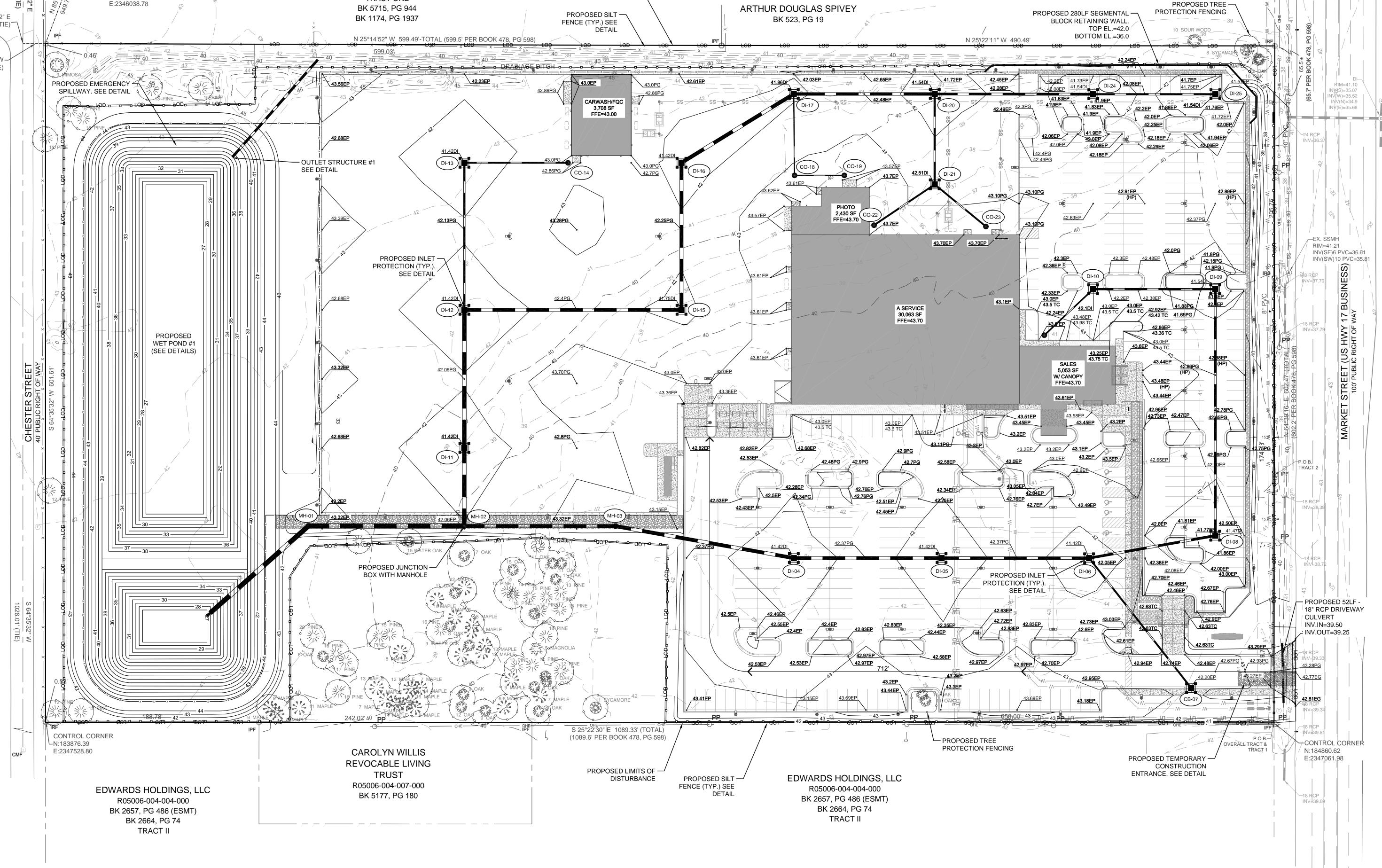
SITE CONTRACTOR SHALL STRIP TOPSOIL AND ANY UNSUITALBE MATERIAL AND PROVIDE CUT/FILL OPERATIONS TO PROVIDE A COMPACTED CONTROLLED BUILDING PAD, IN ACCORDANCE WITH THE SUBSURFACE GEOTECHNICAL EXPLORATION AND TECHNICAL

SPECIFICATIONS.

1. THIS PROJECT DRAINS TO A PROPOSED ON-SITE STORM WET DETENTION BASIN. 2. ONCE SITE IS STABILIZED AND SEDIMENT BASIN IS CONVERTED TO WET DETENTION BASIN, CONTRACTOR SHALL REMOVE ALL SEDIMENT FROM BASIN AND RESTORE TO DESIGN

ELEVATIONS.

- . ALL CONSTRUCTION TO BE IN ACCORDANCE WITH ALL STATE OF NC, CITY OF WILMINGTON, AND NEW HANOVER COUNTY STANDARDS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL PLACE INLET PROTECTION AROUND ALL STORM DRAIN INLETS TO PROTECT THE SYSTEM FROM COLLECTING SEDIMENTATION DURING CONSTRUCTION. INLET PROTECTION SHALL REMAIN IN PLACE UNTIL THE PARKING LOT IS PAVED.
- . CONTRACTOR SHALL ADJUST ALL FRAMES OF EX. UTILITY INFRASTRUCTURE WITHIN ASPHALT OVERLAY AND NEW ASPHALT AREAS TO MATCH PROPOSED GRADES.
- 4. ALL PROPOSED SPOT ELEVATIONS SHOWN IN THE PARKING LOT ARE PROPOSED EDGE OF PAVEMENT OR FACE OF CURB @ GUTTER FOR CURB SECTIONS, UNLESS NOTED OTHERWISE.
- 5. ALL ROOF DRAINS SHALL BE DIRECTED TOWARDS ON-SITE COLLECTION SYSTEM. NO ROOF DRAINS SHALL BE DIRECTED OFF-SITE WITHOUT ENTERING THE ON-SITE COLLECTION FIRST.
- 6. ALL SIDEWALK CROSS SLOPES HAVE BEEN GRADED TO MEET ADA REGULATIONS. CONTRACTOR SHALL CONFIRM GRADES BEFORE PLACING PAVEMENT OR SIDEWALKS AND REPORT ANY DISCREPANCIES TO OWNER AND/OR ENGINEER.
- . THE CONTRACTOR SHALL USE EITHER RCP (CL. III) OR HDPE PIPE FOR THE STORM DRAINAGE SYSTEM. IF THE CONTRACTOR CHOOSES TO USE HDPE PIPE, IT SHALL BE ADS N-12 WT STORM PIPE OR APPROVED EQUAL AND SHALL BE INSTALLED TO MANUFACTURER SPECIFICATIONS. IN ADDITION, THE CONTRACTOR SHALL BE SURE TO PROVIDE CONCRETE COLLARS AROUND EACH F.E.S. TO PREVENT FLOATATION IF HDPE PIPE IS CHOSEN.
- 3. CONTRACTOR SHALL REFER TO THE GEOTECHNICAL ENGINEERING REPORT FOR CARMAX -WILMINGTON, NC (CAPE FEAR), PREPARED BY ECS CAROLINAS, LLP, PROJECT NUMBER: 22.22866-REVISED, AND DATED AUGUST 20, 2015 FOR SITE CONSTRUCTION RECOMMENDATIONS.
- 9. CONTRACTOR SHALL STAKE SILT FENCE ALONG LIMITS OF DISTURBANCE LINE. THE SILT FENCE LINETYPE IS OFFSET ON THE DRAWING FOR CLARITY.



PROPOSED LIMITS OF -

DISTURBANCE

STORM SEWER SCHEDULE:

Upstream	Downstream	Diameter	Upstream Pipe	Downstream	Pipe	Slope	Upstream	Downstream	Pipe
Node	Node	(in)	Invert (ft)	Pipe Invert(ft)	Length (ft)	(%)	Rim Elev (ft)	Rim Elev (ft)	Materia
CO-14	DI-13	15	33.25	32.79	92	0.50	42.80	41.42	CL.III RC
DI-13	DI-12	18	32.79	32.14	130	0.50	41.42	41.42	CL.III RC
DI-12	DI-11	42	32.14	31.52	122	0.50	41.42	41.42	CL.III RC
DI-11	MH-02	42	31.52	31.15	69	0.54	41.42	42.06	CL.III RC
MH-02	MH-01	48	31.15	30.46	138	0.50	42.06	43.25	CL.III RC
MH-01	FES-100	48	30.46	27.50	116	2.54	43.25	38.00	CL.III RC
CO-19	CO-18	8	37.42	37.20	44	0.50	43.51	43.21	CL.III RC
CO-18	DI-17	8	37.20	36.84	72	0.50	43.21	41.86	CL.III RC
DI-17	DI-16	36	34.84	34.25	117	0.50	41.86	41.42	CL.III RC
DI-16	DI-15	36	34.25	33.60	130	0.50	41.42	41.75	CL.III RC
DI-15	DI-12	36	33.60	32.64	192	0.50	41.75	41.42	CL.III RC
CO-22	DI-21	15	36.19	35.86	65	0.50	43.62	42.51	CL.III RC
DI-21	DI-20	18	35.86	35.46	80	0.50	42.51	41.54	CL.III RC
DI-20	DI-17	30	35.46	34.84	125	0.50	41.54	41.86	CL.III RC
CO-23	DI-21	15	36.15	35.86	59	0.50	43.62	42.51	CL.III RC
CO-26	DI-10	12	40.00	35.94	60	6.82	43.49	42.10	CL.III RC
DI-10	DI-09	18	35.94	35.45	108	0.45	42.10	41.54	CL.III RC
DI-09	DI-08	24	35.45	34.36	218	0.50	41.54	41.47	CL.III RC
DI-08	DI-06	24	34.36	33.77	114	0.52	41.47	41.42	CL.III RC
DI-06	DI-05	30	33.77	33.13	131	0.49	41.42	41.42	CL.III RC
DI-05	DI-04	36	33.13	32.46	131	0.51	41.42	41.42	CL.III RC
DI-04	MH-03	36	32.46	31.82	161	0.40	41.42	43.25	CL.III RC
MH-03	MH-02	36	31.82	31.15	134	0.50	43.25	42.06	CL.III RC
DI-07	DI-06	15	34.61	33.77	146	0.57	42.20	41.42	CL.III RC
DI-25	DI-24	18	36.70	36.16	108	0.50	41.54	41.54	CL.III RC
DI-24	DI-20	24	36.16	35.46	141	0.50	41.54	41.54	CL.III RC

LEGEND:				
16 <i></i> _	EXISTING CONTOUR			
29	PROPOSED CONTOUR			
EG=25.05	EXISTING SPOT ELEVATION			
EP=25.05	PROPOSED EDGE OF PAVEMENT			
TW=25.05	PROPOSED SIDEWALK ELEVATION			
SW=25.05	PROPOSED GUTTER FLOW LINE			
TC=25.05	PROPOSED TOP OF CURB ELEVATION			
· · · · · · · · · · · · · · · · · · ·	INLET PROTECTION			
LOD	LIMITS OF DISTURBANCE			
	SILT FENCE			
	TREE PROTECTION FENCING			
~~ ~	DRAINAGE FLOW PATH			
(CB-202)	DRAINAGE INLET LABEL			
	SPILL GUTTER			
	ROCK INLET PROTECTION			
S-01 — ELEV. 33.5	GEOTECH BORING LOCATION			

ΛΕΝΤ	NOTATION: CI = CURB INLET DI = DROP INLET CO = CLEANOUT DDI = DOUBLE DROP INLET MH = STORM DRAIN MANHOLE RD = ROOD DRAIN CLEANOUT FFE = FINISHED FLOOR ELEVATION BPE = BUILDING PAD ELEVATION	
ATION		
INE		
LEVATION		
3		

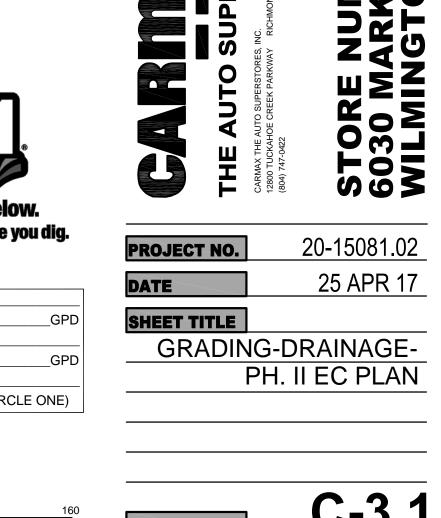
ARTHUR DOUGLAS SPIVEY TRUST R05006-004-009-000

TRACT ONE

NCGS MONUMENT "CAMA"

N:183540.64





NOT RELEASED FOR CONSTRUCTION

REV# DATE DESCRIPTION BY

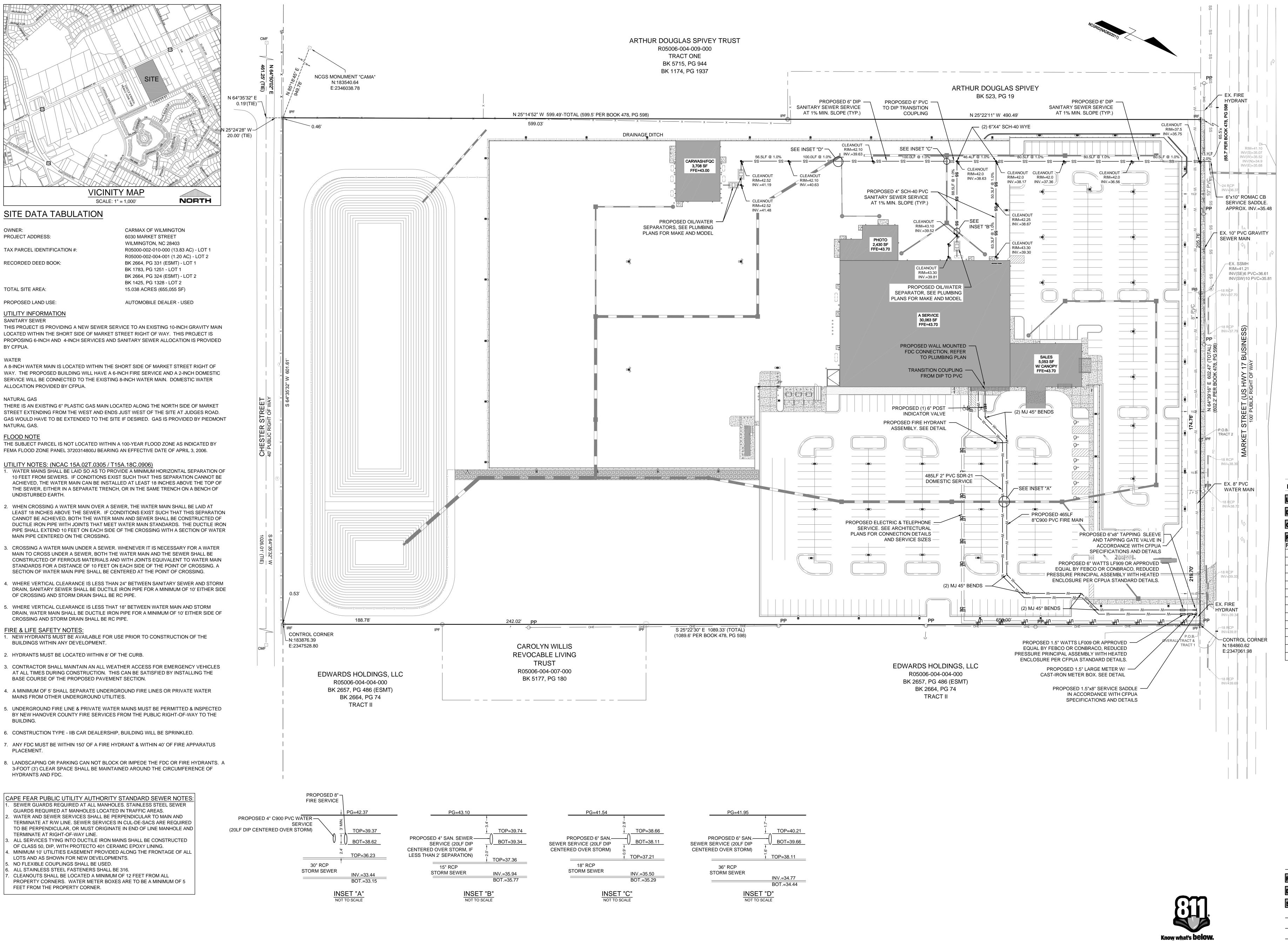
REVISION #1

LATEST DA/PC DA16-026/ PC16-064

DRAWN BY: RPB

CHECKED BY: RPB

09 MAR 17



NOT RELEASED FOR CONSTRUCTION

LATEST DA/PC

DA16-026/ PC16-064

RPB

CHECKED BY:

REVISIONS

REV# DATE DESCRIPTION BY

09 MAR 17 REVISION #1

THE AUTO SUPERSTORE

CARMAX THE AUTO SUPERSTORE

THE AUTO SUPERSTORE

THE AUTO SUPERSTORE

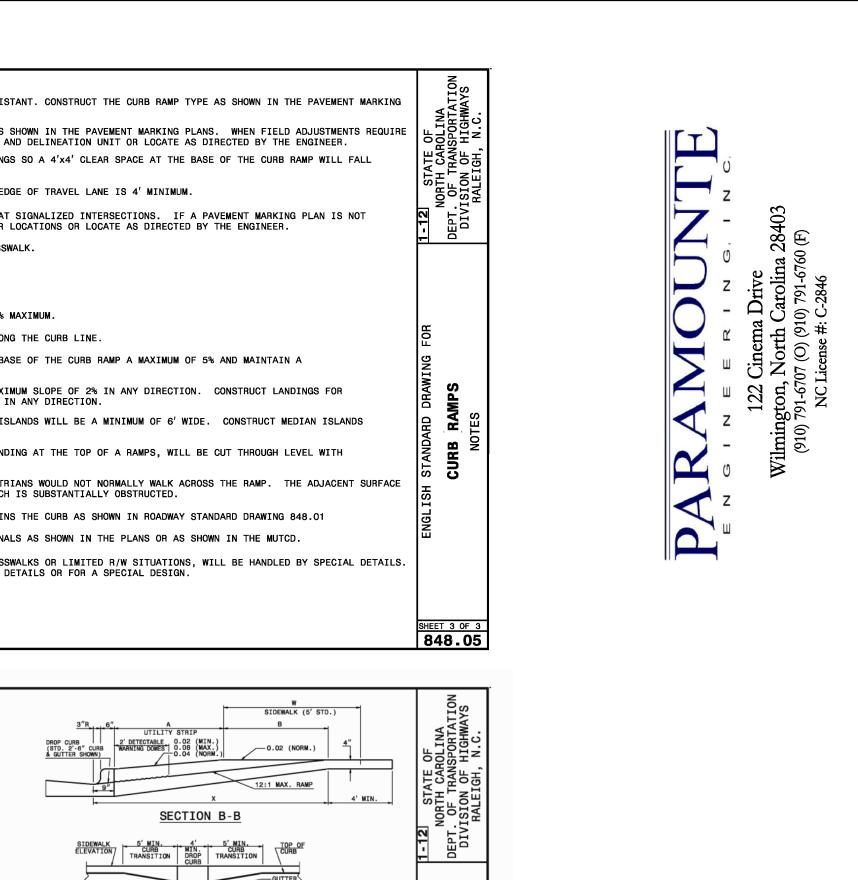
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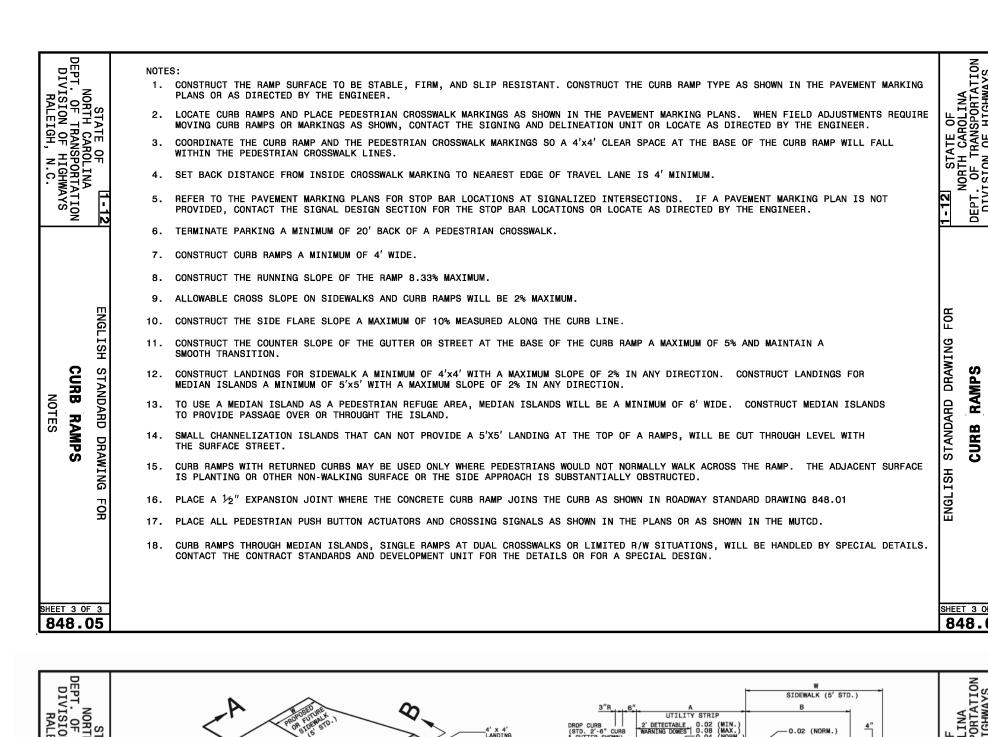
STORE NUMBER 6022

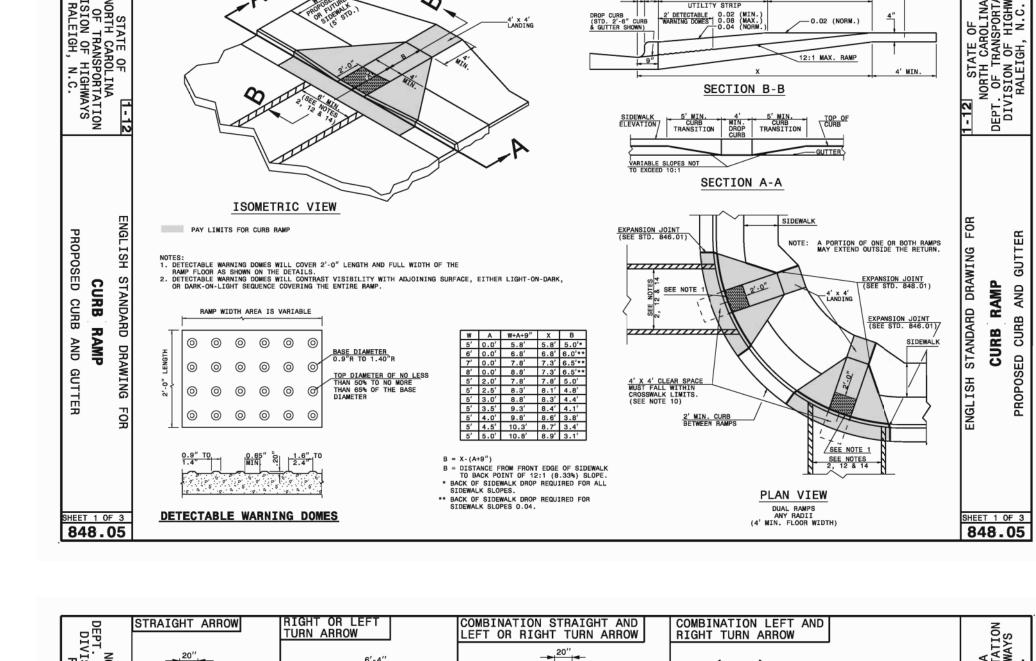
6030 MARKET STREET

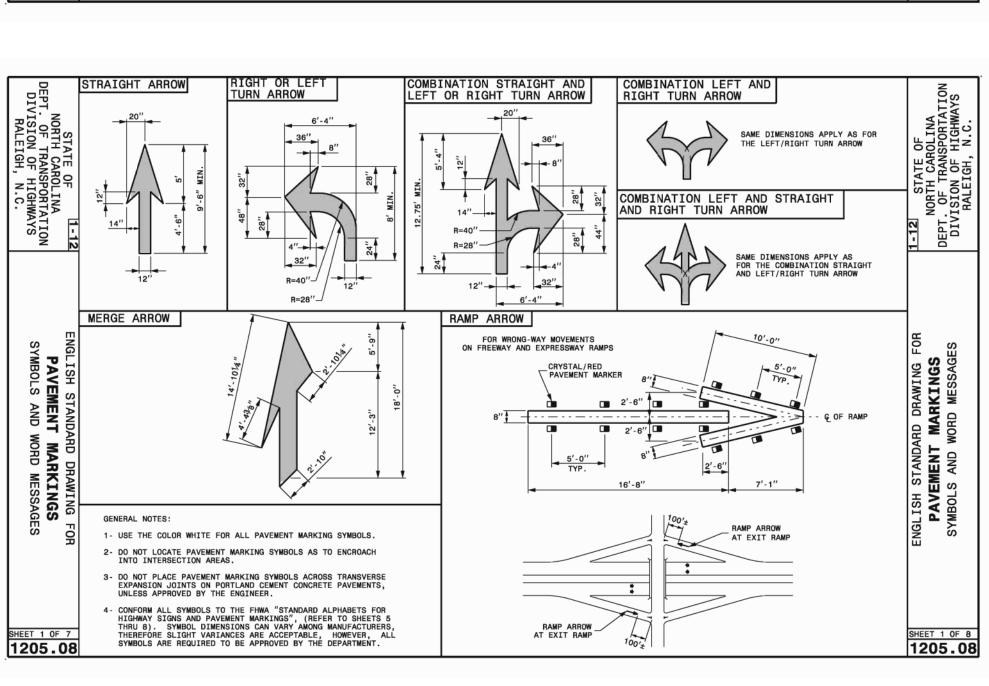
WILMINGTON, NC 28405

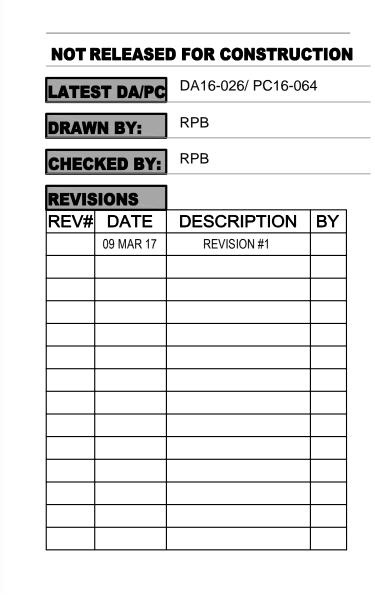
Call before you dig.

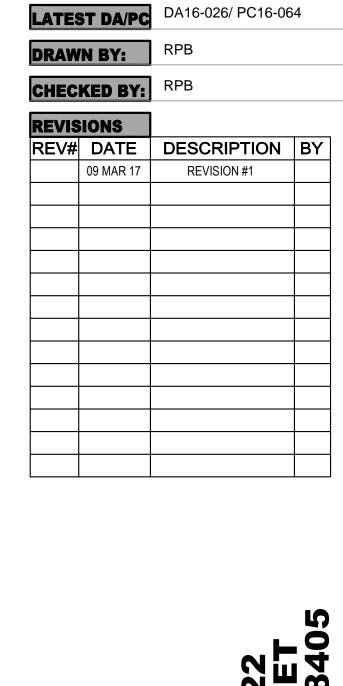


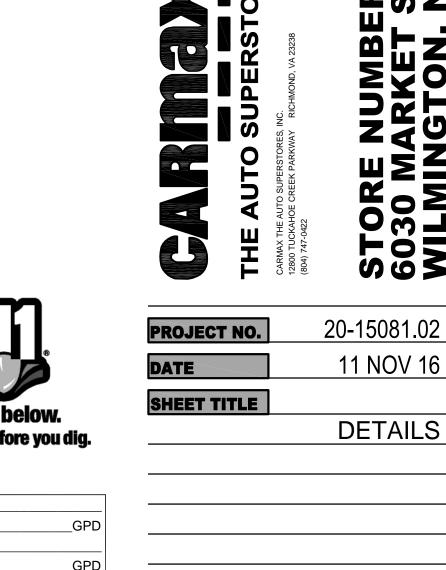




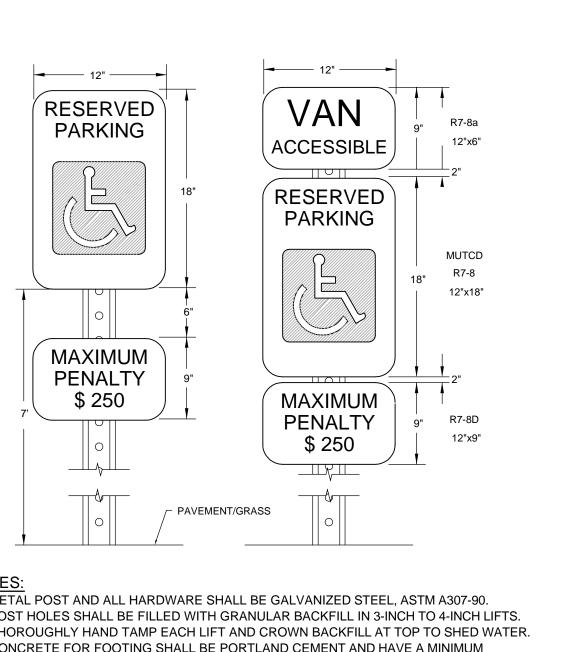








11 NOV 16



HEAVY DUTY PAVEMENT SECTION N.T.S.

1.25" - SF9.5A SURFACE (USE SF9.5B IF

2.5" - I19.0B INTERMEDIATE COURSE

— 6" - B25.0B BASE COURSE

CONSTRUCTION IS BETWEEN MAY AND SEPTEMBER)

— COMPACTED SUBGRADE * 100% ASTM D698 TO 12"

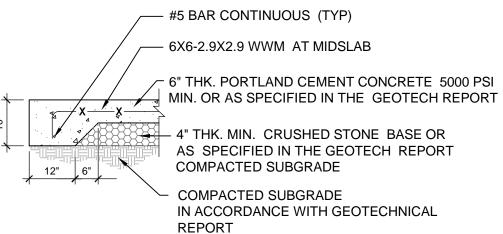
-1.25" - SF9.5A SURFACE (USE SF9.5B IF

- 2.5" - 119.0B INTERMEDIATE COURSE

— 8" - B25.0B BASE COURSE

CONSTRUCTION IS BETWEEN MAY AND SEPTEMBER)

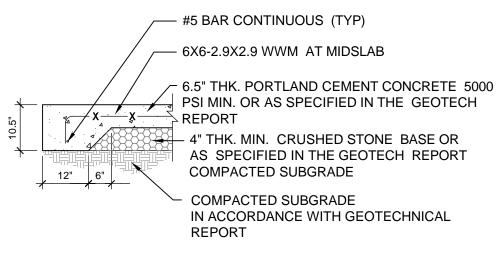
— COMPACTED SUBGRADE * 100% ASTM D698 TO 12"



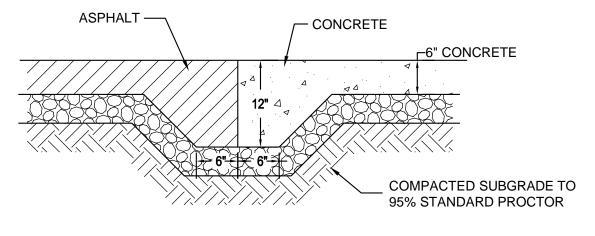
LIGHT DUTY PAVEMENT SECTION

N.T.S.

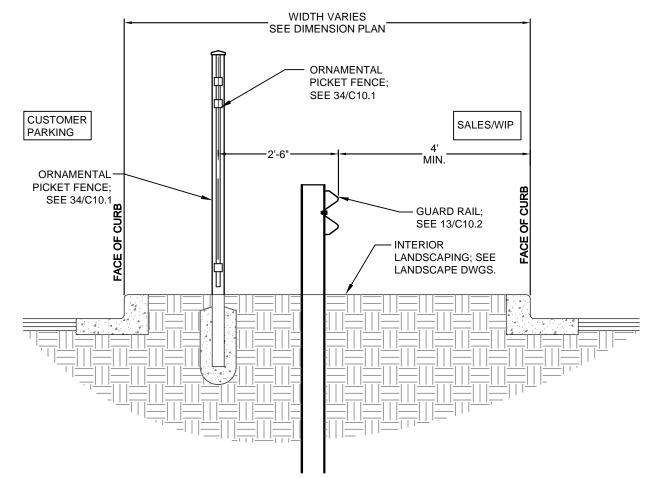
LIGHT-DUTY CONCRETE PAVEMENT SECTION NOT TO SCALE



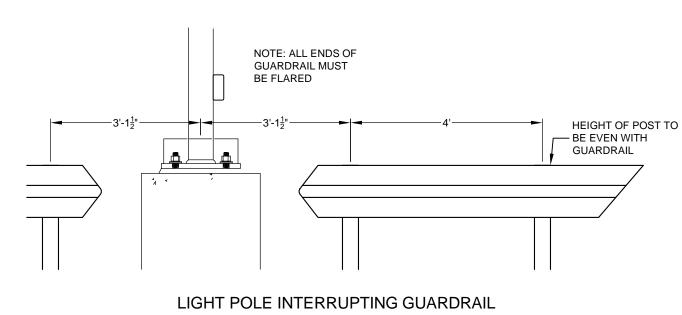
HEAVY-DUTY CONCRETE PAVEMENT SECTION NOT TO SCALE



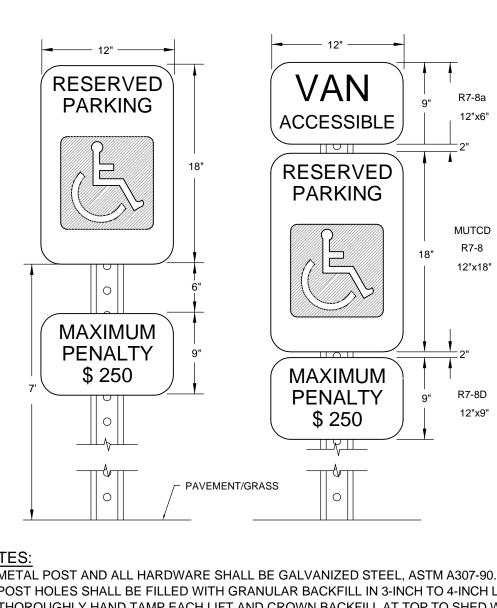
ASPHALT TO CONCRETE PAVEMENT TRANSITION



FENCE/GUARD RAIL SECTION



ELEVATION LIGHT POLE / GUARD RAIL



METAL POST AND ALL HARDWARE SHALL BE GALVANIZED STEEL, ASTM A307-90. 2. POST HOLES SHALL BE FILLED WITH GRANULAR BACKFILL IN 3-INCH TO 4-INCH LIFTS. 3. THOROUGHLY HAND TAMP EACH LIFT AND CROWN BACKFILL AT TOP TO SHED WATER. 4. CONCRETE FOR FOOTING SHALL BE PORTLAND CEMENT AND HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3000 PSI.

5. SIGNS SHALL BE CONSTRUCTED OF ALUMINUM, TYPE 6061-T6 AND IN CONFORMANCE WITH ASTM STD B-209. THICKNESS SHALL BE 0.80 INCHES. 6. TOP EDGE TREATMENT FILM SHALL BE 3" WIDE, CLEAR AND TRANSPARENT WITH A SUB-RESISTANT PRESSURE SENSITIVE NON-YELLOWING ADHESIVE, "SKOTCHCAL" TRANSPARENT FILE #639.

7. REFLECTIVE FACING MATERIAL SHALL BE SCOTCHLITE HIGH INTENSITY, MANUFACTURED BY 3M COMPANY 8. SIGN LETTERING SHALL BE NPS MODIFIED CLARENDON TYPEFACE. UPPERCASE LETTERS SHALL BE 3.75" HEIGHT AND LOWERCASE LETTERS SHALL BE 2.5" HEIGHT WITH 3.75" SPACING BETWEEN LINES.

9. CORNER RADIUS OF SIGNS SHALL BE 2.5" ARROWS SHALL BE 5.625" LONG BY 3.75" HIGH. 10. MOUNT ON BUILDING OR METAL POST AS DIRECTED.

HANDICAP SIGN DETAIL

NOT TO SCALE

— HOLD POST FLUSH WITH

GALV. HIGHWAY GUARD

RAIL WITH FLARED ENDS

CHANGES IN DIRECTION

— FIELD WELD NUT TO BOLT

─ W6 x 9 GALV. STL. POST

- FINISHED GRADE

— FACE OF CURB

SALES/WIP

- SITE WALL, SEE ARCHITECTURAL SITE

- AREA TO REMAIN

LANDSCAPING AND

EQUIPMENT; SEE

LANDSCAPE DWGS

CLEAR FROM

DETAILS

GUARD RAIL N.T.S.

SECTION - SALES/WALL

SPACING AT GUARDRAIL POST

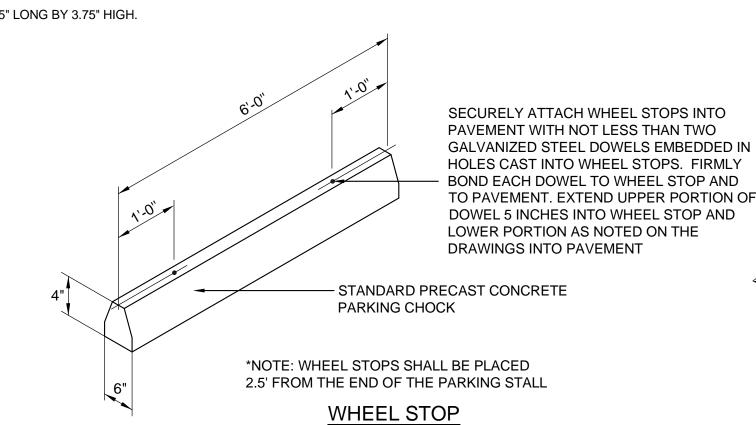
DETAIL - GUARDRAIL POST SPACING

4' MAXIMUM BETWEEN POST AT

ANY GUARDRAIL OPENING,

AT ALL TERMINATION AND

TOP OF GUARD RAIL



W/ ASPHALT

HANDICAP RAMP

NOT TO SCALE

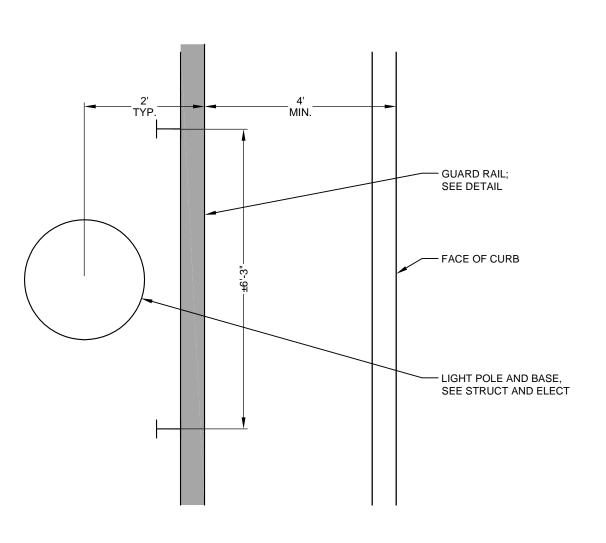
WHEEL CHAIR RAMP (DROP CURB)

NOT TO SCALE

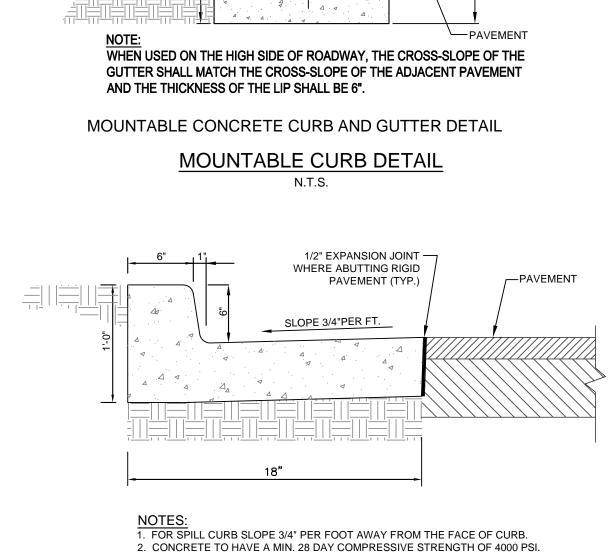
SLOPE

-PAVEMENT

WHEEL STOP NOT TO SCALE



LIGHT POLE/GUARD RAIL/CURB PLAN - SALES & WIP PARKING



TRANSITION DIMENSIONS VARY SEE SITE PLAN FOR LENGTH

TRANSITION CURB DETAIL

TYPICAL SIDEWALK DETAIL

TYPICAL SIDEWALK @ CURB

– CONT SCORED JOINT @ ±10'-0" O.C.; 1/4 DEPTH OF

— COARSE AGGREGATE

__ 1/2" EXPANSION JOINT WITH

COMPRESSIBLE JOINT FILLER

— CONC. SLAB

@ <u>+</u>30" OC.

- COARSE AGGREGATE

— CONC. SLAB;

—— 1/2" JOINT FILLER

6" CURB W/ GUTTER

BITUMASTIC ASPHALT;

SEE CIVIL DWGS.

SIDEWALK, OPTIONAL

- CURB & GUTTER

— FLUSH CONDITION

- PARKING / ROAD SURFACE

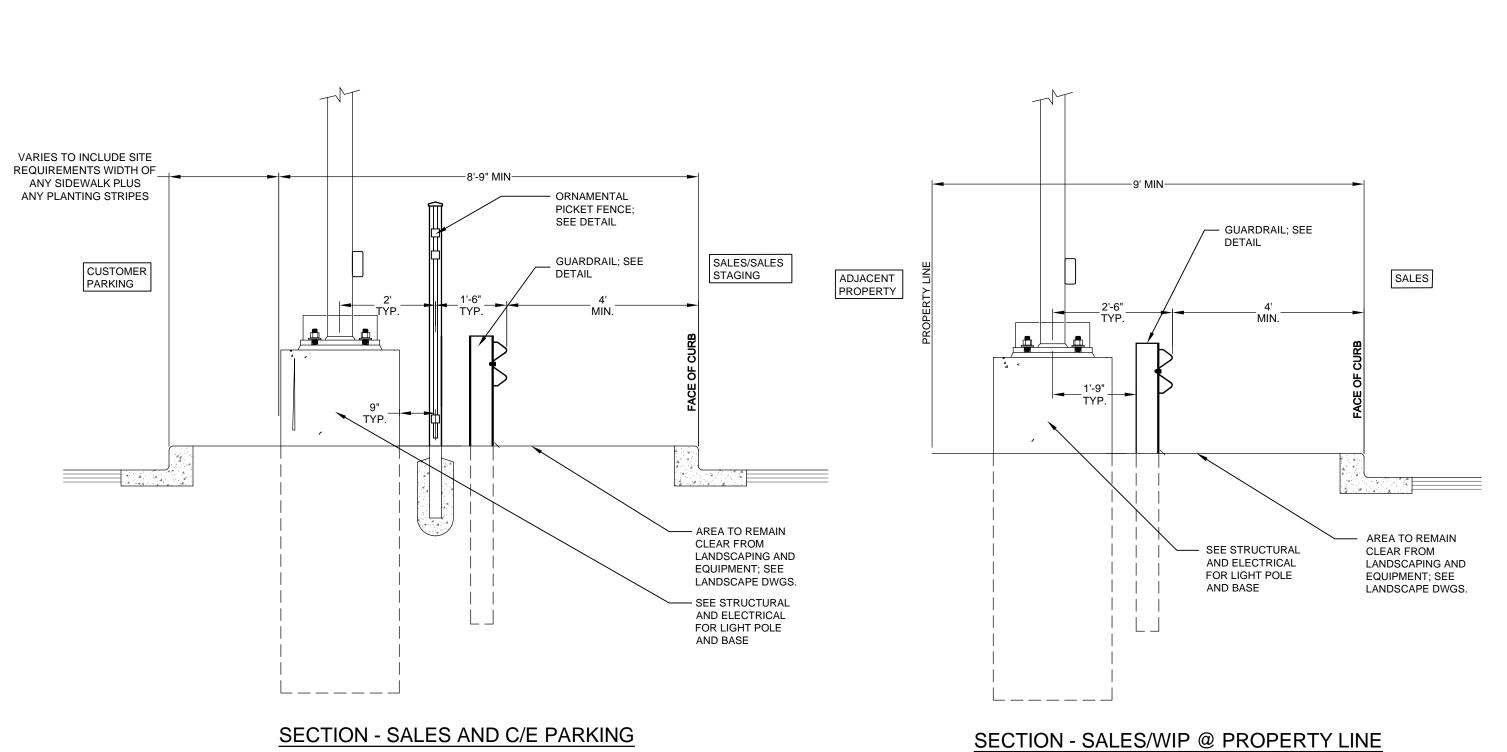
CURB FACE -

6" HIGH CURB

TRANSITIONS DOWN TO —

3. USE CURB SPECIFIED BY LOCAL GOVERNING AUTHORITY IN PUBLIC R/W. STANDARD CONCRETE CURB & GUTTER

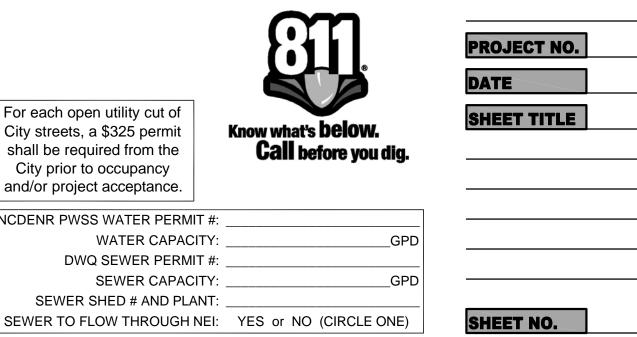
N.T.S.

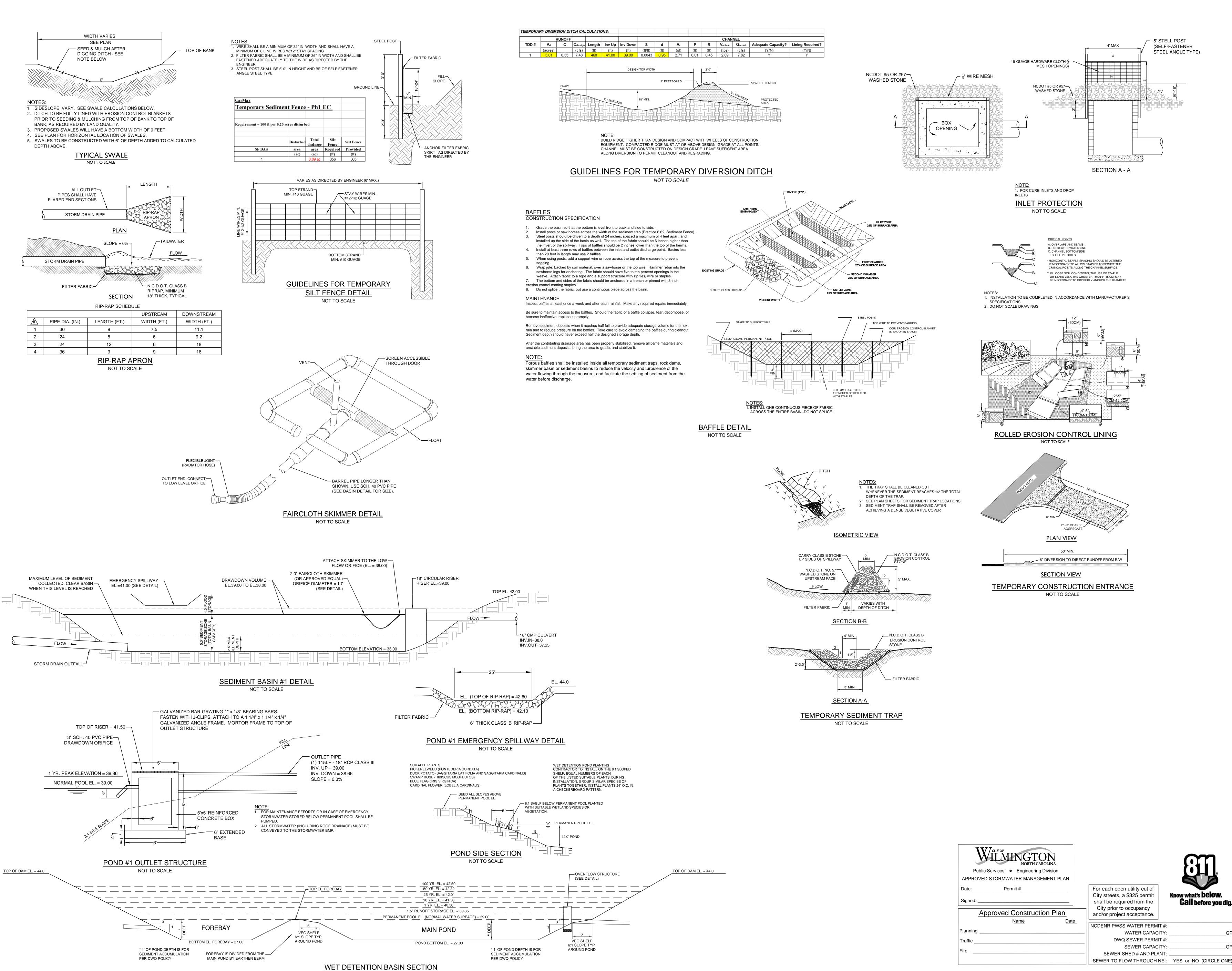




For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance. NCDENR PWSS WATER PERMIT # WATER CAPACITY: DWQ SEWER PERMIT #: SEWER CAPACITY:

SEWER SHED # AND PLANT:



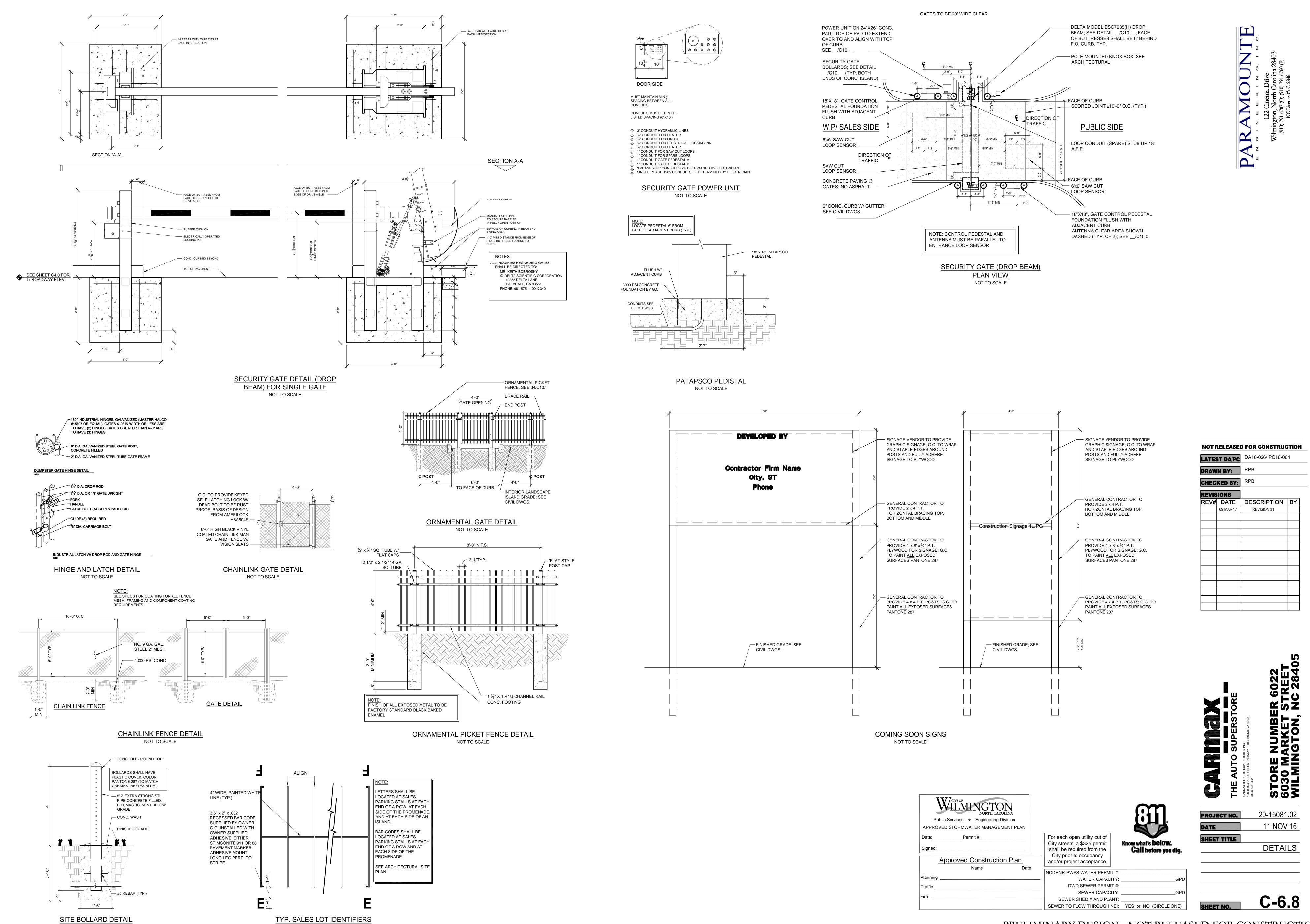


NOT TO SCALE

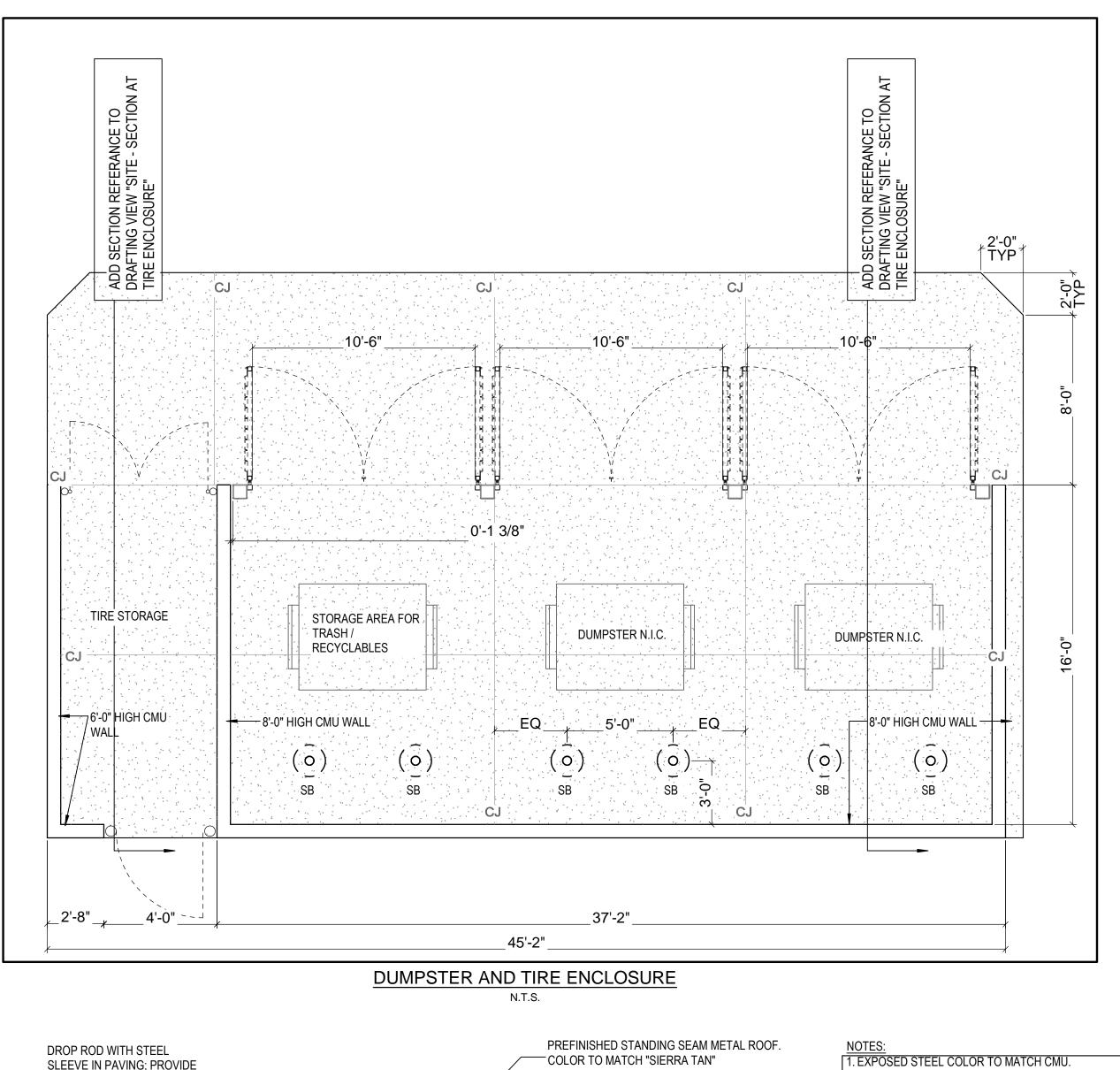
NOT RELEASED FOR CONSTRUCTION DRAWN BY: CHECKED BY: RPB REV# DATE | DESCRIPTION BY 09 MAR 17 **REVISION #1**

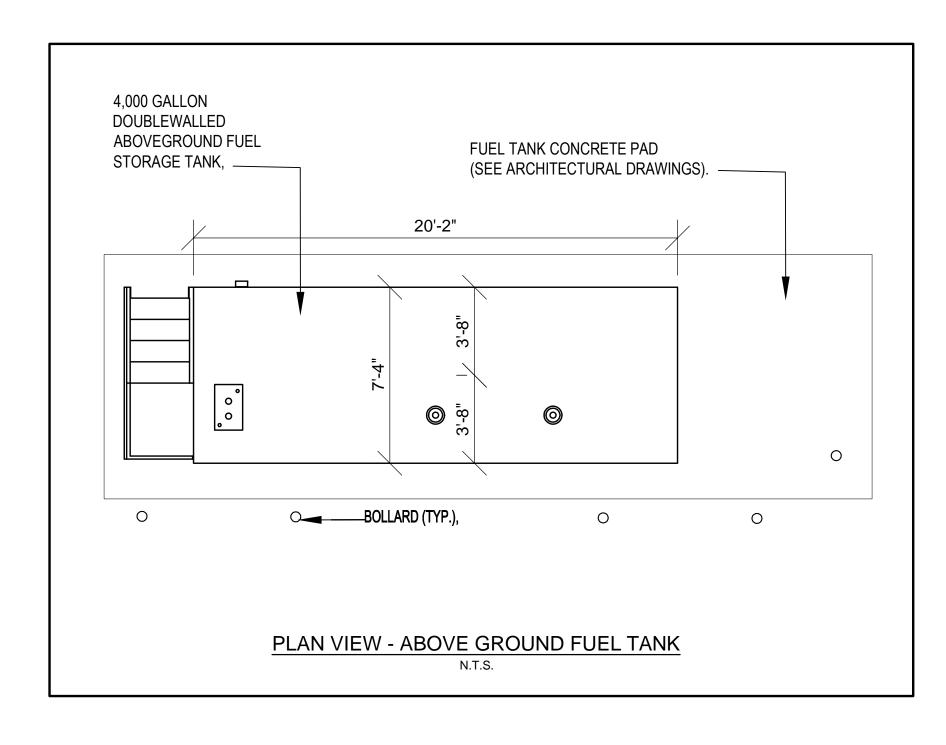
20-15081.02 PROJECT NO. 11 NOV 16 SHEET TITLE **DETAILS**

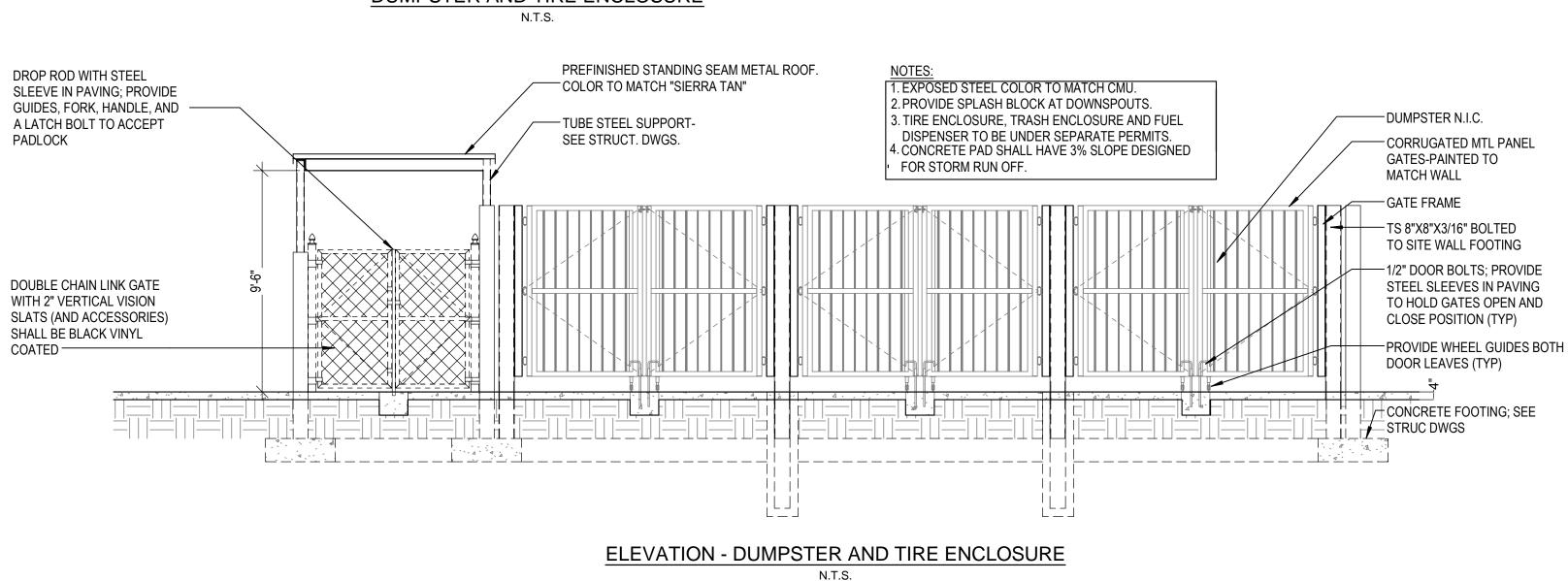
PRELIMINARY DESIGN - NOT RELEASED FOR CONSTRUCTION

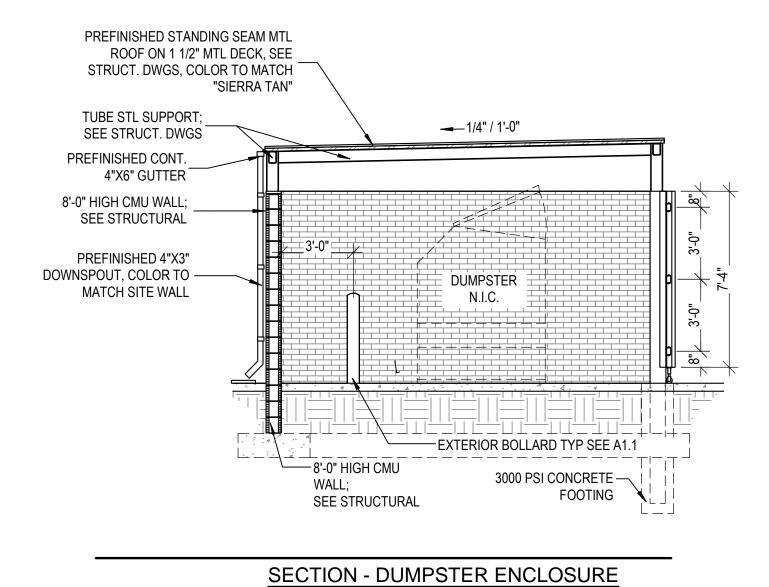


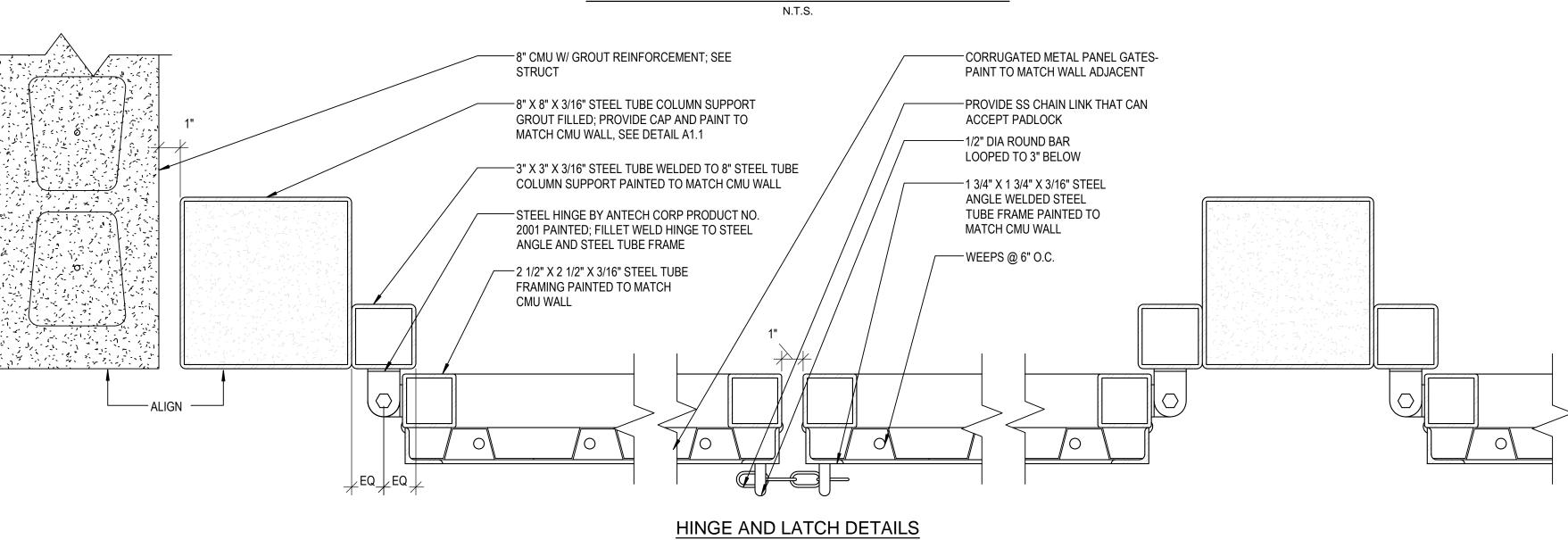
NOT TO SCALE

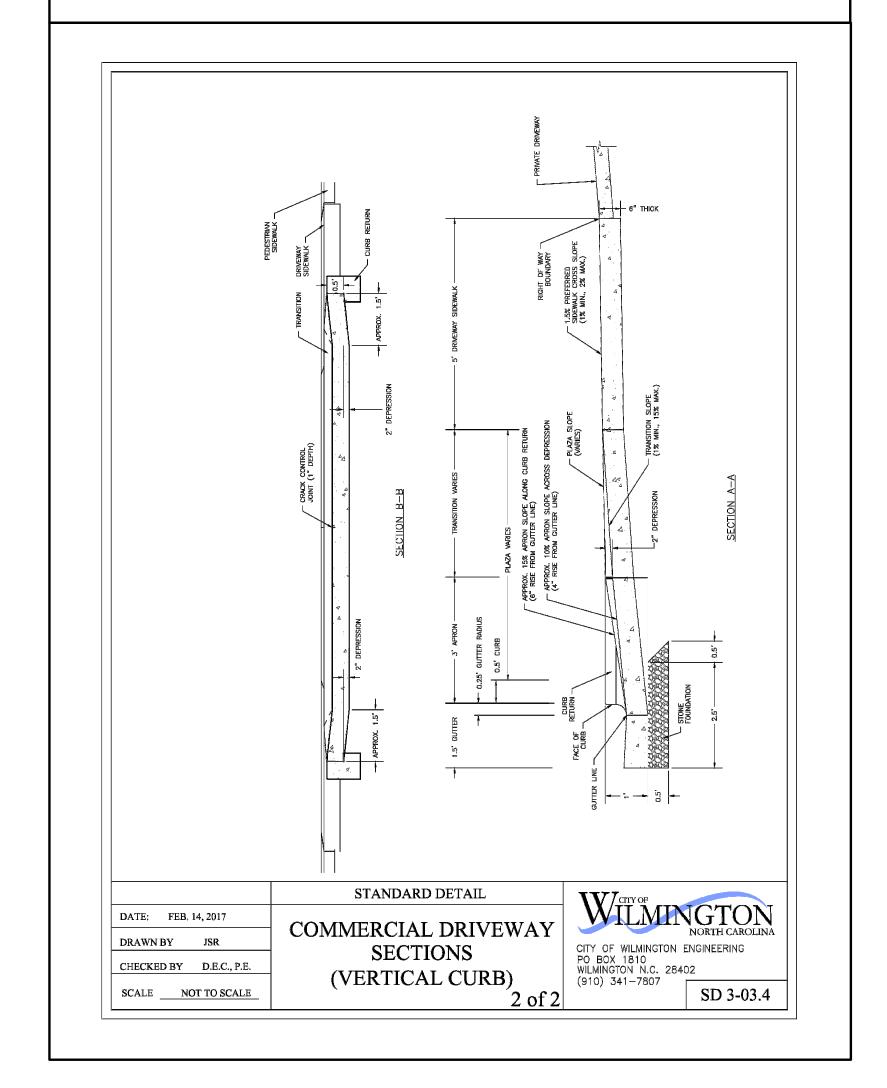












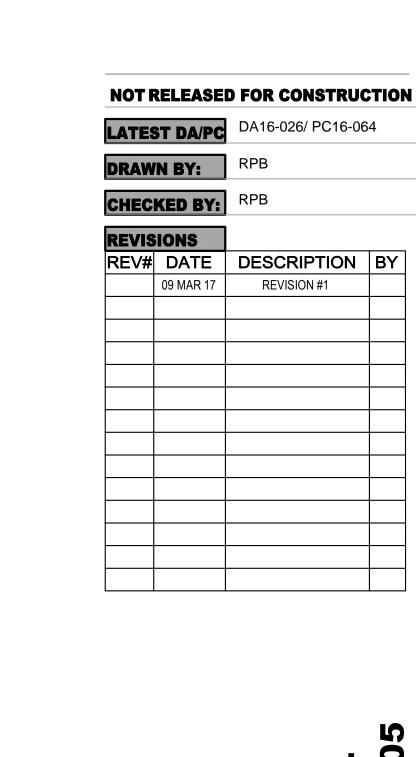
STANDARD DETAIL

PLAN (VERTICAL CURB)

SCALE NOT TO SCALE

CITY OF WILMINGTON ENGINEERING
PO BOX 1810
WILMINGTON N.C. 28402
(910) 341-7807

SD 3-03.3



ST0 6030 WILI

20-15081.02

11 NOV 16

DETAILS

C-6.9

