



**COORDINATION NOTES:**

- 1. THE CONTRACTOR IS REQUIRED TO OBTAIN ANY/ALL PERMITS REQUIRED FOR CONSTRUCTION OF THESE PLANS.
2. ALL CONSTRUCTION TO BE IN ACCORDANCE WITH PERMITS ISSUED AND WITH THE CITY OF WILMINGTON, NEW HANOVER COUNTY, CAPE FEAR PUBLIC UTILITY AUTHORITY (CPFA), AND THE STATE OF NORTH CAROLINA.
3. THE CONTRACTOR IS TO ESTABLISH AND CHECK ALL HORIZONTAL AND VERTICAL CONTROLS TO BE USED WITH THE PROJECT.
4. ANYTIME WORK IS PERFORMED OFF-SITE OR WITHIN AN EXISTING EASEMENT, THE CONTRACTOR IS TO NOTIFY THE HOLDER OF SAID EASEMENT AS TO THE NATURE OF PROPOSED WORK, AND TO FOLLOW ANY GUIDELINES OR STANDARDS WHICH ARE ASSOCIATED WITH OR REFERENCED IN THE RECORDED EASEMENT.
5. CONTRACTOR SHALL REFER TO ARCHITECTURAL DRAWINGS BY OTHERS FOR ALL BUILDING DIMENSIONS AND DETAILS.

**GENERAL NOTES:**

- 1. EXISTING CONDITIONS AND TOPOGRAPHIC SURVEY COMPLETED BY PARAMOUNT ENGINEERING, INC. THE SURVEY SHALL BE FIELD VERIFIED BY CONTRACTOR AND ANY DISCREPANCIES REPORTED TO THE OWNER, OWNERS REPRESENTATIVE AND/OR ENGINEER IMMEDIATELY.
2. REASONABLE CARE HAS BEEN EXERCISED IN SHOWING THE LOCATION OF EXISTING UTILITIES ON THE PLANS. THE EXACT LOCATION OF ALL EXISTING UTILITIES IS NOT KNOWN IN ALL CASES. THE CONTRACTOR SHALL EXPLORE THE AREA AROUND OF DITCHING OPERATIONS BY OBSERVATIONS, ELECTRONIC DEVICES, HAND DIGGING AND BY PERSONAL CONTACT WITH THE UTILITY COMPANIES.
3. CONTRACTOR SHALL COMPLY WITH THE LATEST REVISIONS AND INTERPRETATIONS OF THE DEPARTMENT OF LABOR SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION PROMULGATED UNDER THE OCCUPATIONAL SAFETY AND HEALTH ACT.
4. CONTRACTOR SHALL PLAN AND CONSTRUCT WORK SO AS TO CAUSE MINIMUM INCONVENIENCE TO THE OWNER AND THE PUBLIC.
5. ALL MATERIAL CLEARED OR DEMOLISHED BY THE CONTRACTOR IN ORDER TO CONSTRUCT THE WORK SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE PROPERLY DISPOSED OF OFF-SITE.
6. ALL WORK BY THE CONTRACTOR SHALL BE WARRANTED BY THE CONTRACTOR FOR A PERIOD OF ONE YEAR AFTER THE OWNER ACCEPTS THE WORK.
7. CONTRACTOR SHALL CALL THE NORTH CAROLINA ONE-CALL CENTER AT 1 (800) 632-4949 AND ALLOW THE CENTER TO LOCATE EXISTING UTILITIES BEFORE DIGGING.

**DEMOLITION NOTES:**

- 1. CONTRACTOR TO COORDINATE WITH THE OWNER TO PROPERLY MAINTAIN OR RELOCATE EXISTING SERVICE CONNECTIONS WHEN NECESSARY.
2. CONTRACTOR IS TO WALK THE SITE AND BECOME FAMILIAR WITH THE SCOPE OF DEMOLITION REQUIRED.
3. DEMOLITION SHALL INCLUDE BUT IS NOT LIMITED TO THE EXCAVATION, HAULING AND OFFSITE DISPOSAL OF CONCRETE PADS, CONCRETE DITCHES, FOUNDATIONS, SLABS, STEPS, AND STRUCTURES.
4. THE CONTRACTOR SHALL PROTECT ALL ADJACENT PROPERTY, STRUCTURES AND UTILITIES ON THE PROPERTY NOT TO BE DEMOLISHED.
5. ELECTRIC, TELEPHONE, SANITARY SEWER, WATER AND STORM SEWER UTILITIES THAT SERVICE OFF-SITE PROPERTIES SHALL BE MAINTAINED DURING THE CONSTRUCTION PROCESS BY THE CONTRACTOR.
6. THE CONTRACTOR SHALL PRODUCE A PHOTOGRAPHIC RECORD (DIGITAL) OF DEVELOPMENT COMMENCING WITH A RECORD OF THE SITE AS IT APPEARS BEFORE DEMOLITION HAS BEGUN.
7. EXISTING CURB AND GUTTER, LIGHTS, SIDEWALK, AND UTILITIES NOT INTENDED FOR DEMOLITION SHALL BE MAINTAINED, PROTECTED AND UNDISTURBED DURING DEMOLITION.
8. ALL EXISTING IMPROVEMENTS INDICATED OR REQUIRED TO BE DEMOLISHED SHALL INCLUDE REMOVAL FROM THE PROPERTY AND PROPER DISPOSAL.
9. CONTRACTOR SHALL COORDINATE RELOCATION OF ALL EXISTING OVERHEAD AND UNDERGROUND UTILITIES INCLUDING CABLE, GAS, TELEPHONE AND ELECTRIC AND ANY OTHER UTILITIES THROUGH THE SITE WITH THE RESPECTIVE COMPANIES.
10. PROVIDE SMOOTH SAW CUT OF EXISTING PAVEMENTS, CURBS AND GUTTERS AND SIDEWALKS TO BE DEMOLISHED.
11. ALL DEMOLITION WORK SHALL BE DONE IN STRICT ACCORDANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS AS WELL AS OSHA REGULATIONS.
12. EXISTING FIRE HYDRANTS ARE TO REMAIN IN SERVICE.
13. INFORMATION CONCERNING UNDERGROUND UTILITIES WAS OBTAINED FROM AVAILABLE RECORDS, BUT THE CONTRACTOR MUST DETERMINE THE EXACT LOCATION AND ELEVATIONS OF THE MAINS BY DIGGING TEST PITS BY HAND.

**TRAFFIC NOTES:**

- 1. ALL PAVEMENT MARKINGS IN PUBLIC RIGHTS-OF-WAY & FOR DRIVEWAYS) ARE TO BE THERMOPLASTIC & MEET NCDOT STANDARDS.
2. TRAFFIC CONTROL DEVICES (INCLUDING SIGNS AND PAVEMENT MARKINGS) IN AREAS OPEN TO PUBLIC TRAFFIC ARE TO MEET MUTCD (MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES) STANDARDS.
3. ALL TRAFFIC CONTROL SIGNS AND MARKINGS NOT WITHIN THE PUBLIC RIGHT-OF-WAY ARE TO BE MAINTAINED BY THE PROPERTY OWNER IN ACCORDANCE WITH MUTCD STANDARDS.
4. ALL PARKING STALL MARKINGS AND LANE ARROWS WITHIN THE PARKING AREAS SHALL BE WHITE.
5. ANY OPEN CUTTING OF A NCDOT STREET REQUIRES AN ENCROACHMENT PERMIT.
6. TACTILE WARNING MATS TO BE INSTALLED AT ALL WHEELCHAIR RAMPS.
7. A LANDSCAPING PLAN, INDICATING THE LOCATION OF REQUIRED STREET-YARD PLANTINGS SHALL BE PROVIDED FOR REVIEW OF SIGHT DISTANCES.
8. ALL SIGNS AND PAVEMENT MARKINGS IN AREAS OPEN TO PUBLIC TRAFFIC ARE TO MEET MUTCD STANDARDS.

**EROSION CONTROL AND SEQUENCE OF CONSTRUCTION NOTES:**

NOTE: THESE EROSION CONTROL AND SEQUENCE OF CONSTRUCTION NOTES ARE INTENDED FOR EACH PHASE OF CONSTRUCTION. THE ORDER AND STEPS TAKEN MUST BE IMPLEMENTED AS EACH PART OF THE PROJECT IS DEVELOPED, WHETHER AS A WHOLE OR IN PHASES. ANY EROSION CONTROL DEVICES MEASURES MUST REMAIN IN PLACE UNTIL THE ENTIRE BASIN DESIGNED TO HANDLE SEDIMENTATION AND EROSION IS STABILIZED AND ALL IMPROVEMENTS WITHIN THE BASIN ARE COMPLETE.

- 1. CONSTRUCT TEMPORARY GRAVEL CONSTRUCTION ENTRANCES(S), ESTABLISH THE LIMITS OF DISTURBANCE, AND TEMPORARY SILT FENCE.
2. CLEAR AND REMOVE FROM SITE ALL TREES (ONLY ONES TO BE REMOVED), ROOTS, ROOT MAT, ETC. FROM THE AREA WITHIN THE DESIGNATED CLEARING LIMITS.
3. INSTALL REMAINING EROSION CONTROL MEASURES AS SHOWN ON THE PLANS WITHIN THE AREA DISTURBED.
4. PLANT GRASS OVER ALL GRADED AREAS WITHIN 15 WORKING DAYS OF CEASE OF ANY GRADING ACTIVITY.
5. IMMEDIATELY UPON THE INSTALLATION OF ANY STORM DRAINAGE CATCH BASIN, DROP INLET, ETC., THE CONTRACTOR SHALL INSTALL INLET PROTECTION TO STOP ANY SEDIMENT FROM ENTERING THE DRAINAGE SYSTEM.
6. IMMEDIATELY UPON THE INSTALLATION OF ANY STORM DRAINAGE SYSTEM, THE CONTRACTOR SHALL INSTALL OUTLET PROTECTION AS SHOWN ON THE PLANS AND DETAILS.
7. THE CONTRACTOR SHALL CONVERT THE SEDIMENT BASIN TO THE WET DETENTION POND ONCE THE ENTIRE DRAINAGE BASIN IS FULLY STABILIZED.
8. DURING GRADING AND AFTER GRADING HAS BEEN COMPLETED, THE CONTRACTOR SHALL CONTINUE TO MAINTAIN PERMANENT AND TEMPORARY EROSION CONTROL MEASURES UNTIL FINAL APPROVAL.
10. UPON RECEIVING FINAL APPROVAL, THE CONTRACTOR CAN REMOVE TEMPORARY EROSION CONTROL MEASURES.
11. THE CONTRACTOR SHALL CONTINUE TO WATER, FERTILIZE, MOW AND MAINTAIN SPRIGGED, SOODED, AND PLANTED AREAS UNTIL ALL CONSTRUCTION IS COMPLETE.

**GENERAL EROSION AND SEDIMENT CONTROL NOTES:**

- 1. THE EROSION CONTROL PLAN SHALL INCLUDE PROVISIONS FOR GROUNDCOVER ON ALL EXPOSED PERIMETER DICES, SWALES, DITCHES, PERIMETER SLOPES AND ALL SLOPES STEEPER THAN 3:1 WITHIN 1 CALENDAR DAYS FROM THE LAST LAND DISTURBING ACTIVITY.
2. UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE NORTH CAROLINA EROSION AND SEDIMENT CONTROL HANDBOOK.
3. THE CONTRACTOR SHALL NOTIFY PLAN APPROVING AUTHORITY ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITY, AND ONE WEEK PRIOR TO FINAL INSPECTION.
4. ALL EROSION AND SEDIMENT CONTROL MEASURES TO BE PLACED PRIOR TO CLEARING AND/OR LAND DISTURBANCE.
5. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN AND PERMIT SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
6. PRIOR TO COMMENCING LAND DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING, BUT NOT LIMITED TO, OFF-SITE DRIVEWAYS OR WASTE AREAS STAGING OR STORAGE AREAS), THE CONTRACTOR SHALL PREPARE AND SUBMIT A SUPPLEMENTARY EROSION CONTROL PLAN TO THE OWNER FOR REVIEW AND TO NEW HANOVER COUNTY FOR APPROVAL.
7. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY EITHER THE REVIEWING AGENCY OR THE ENGINEER.
8. ALL DISTURBED AREAS ARE TO DRAIN TO APPROVED SEDIMENT CONTROL MEASURES AT ALL TIMES DURING LAND DISTURBING ACTIVITIES AND DURING SITE DEVELOPMENT UNTIL FINAL STABILIZATION IS ACHIEVED.
9. ALL AREAS DISTURBED BY CONSTRUCTION UNLESS OTHERWISE IMPROVED SHALL BE SOODED OR SEEDED AS INDICATED AND STABILIZED.
10. DURING Dewatering OPERATIONS, WATER SHALL BE PUMPED INTO AN APPROVED FILTERING DEVICE PRIOR TO DISCHARGE TO RECEIVING OUTLET.

**GENERAL EROSION AND SEDIMENT CONTROL NOTES (CONT.):**

- 1. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF-PRODUCING EVENT.
2. ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED BY CONTRACTOR ONCE STABILIZATION OR A SUFFICIENT GROUND COVER HAS BEEN ESTABLISHED OR AS DIRECTED BY THE ENGINEER.
3. TEMPORARY GRAVEL CONSTRUCTION ENTRANCE SHALL BE REQUIRED AT ALL CONSTRUCTION STAGING AREA ENTRANCES AND ALL CONSTRUCTION ACCESS LOCATIONS INTO NON-PAVED AREA.
4. WHEN CROSSING CREEK OR DRAINAGE WAY, THE DIVISION OF WATER QUALITY SHALL BE CONTACTED PRIOR TO DISTURBING A CREEK.
5. WHEN CROSSING CREEK WITH RIP-RAP WITH FABRIC ALONG DISTURBED BANKS AND CHANNEL, RESTORE SLOPES TO ORIGINAL CONTOURS, BUT NOT STEEPER THAN 2:1 MAXIMUM.
6. MAINTENANCE PLAN:

- 1. ALL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CHECKED FOR STABILITY AND OPERATION FOLLOWING EVERY RUNOFF-PRODUCING RAINFALL, BUT IN NO CASE LESS THAN ONCE EVERY WEEK.
2. ALL CONSTRUCTION ENTRANCES WILL BE PERIODICALLY TOP DRESSED WITH AN ADDITIONAL 2 INCHES OF #4 STONE TO MAINTAIN PROPER DEPTH.
3. SEDIMENT WILL BE REMOVED BEHIND THE SEDIMENT FENCE WHEN IT BECOMES 0.5 FEET DEEP AT THE FENCE.
4. ALL SEEDED AREAS WILL BE FERTILIZED, RESEEDDED AS NECESSARY, AND MULCHED ACCORDING TO SPECIFICATIONS IN THE VEGETATIVE PLAN TO MAINTAIN A VIGOROUS, DENSE VEGETATIVE COVER.
5. INLET PROTECTION - INSPECT WIRE AND ROCK INLET PROTECTION AT LEAST ONCE A WEEK AND AFTER EACH SIGNIFICANT INCH OR GREATER RAINFALL EVENT.
6. SEDIMENT BASIN - REMOVE SEDIMENT AND RESTORE THE BASIN TO ITS ORIGINAL DIMENSIONS WHEN IT ACCUMULATES TO WITHIN ONE HALF OF THE DESIGN DEPTH.
7. SKIMMER - INSPECT SKIMMER AT LEAST ONCE A WEEK AND AFTER EACH RAINFALL TO MAKE SURE THAT THE INTAKE MECHANISM, ORIFICE, OR DISCHARGE PIPE IS NOT CLOGGED WITH TRASH OR SEDIMENT.
8. OUTLET PROTECTION - INSPECT RIP RAP OUTLET STRUCTURES WEEKLY AND AFTER SIGNIFICANT INCH OR GREATER RAINFALL EVENTS.
9. EMERGENCY SPILLWAY / FOREBAY PROTECTION - AFTER EVERY HIGH-WATER EVENT INSPECT THE INTEGRITY OF THE LINED SPILLWAY AND THE ADJACENT EARTHEN BANKS.

**PERMANENT SEEDING SCHEDULE - MIXTURE 5CP**

Table with 3 columns: Species, Rate (lb/acre), and Mixture. Includes Centipede Grass at 10-20 lb/acre.

Table with 2 columns: Seeding Dates and Soil Amendments. Seeding dates listed as March - June. Soil amendments include lime and fertilizer.

Table with 2 columns: Mulch and Maintenance. Mulch listed as DO NOT MULCH. Maintenance listed as Fertilize very sparingly.

Table with 2 columns: Seeding Mixture and Rate (lb/acre). Includes Ryegrass and Annual Lespedeza.

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**NC ACCESSIBILITY NOTES:**

- 1. SPECIAL ATTENTION SHALL BE GIVEN TO COMPLIANCE WITH AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS), THE NORTH CAROLINA BUILDING CODE/ANSI A117.1, AND APPLICABLE LOCAL LAWS & REGULATIONS.
2. IT IS RECOMMENDED THAT CONTRACTORS ARE AWARE OF THE SITE ACCESSIBILITY REQUIREMENTS.
3. THE CONTRACTOR SHALL NOTIFY PARAMOUNT ENGINEERING IMMEDIATELY OF ANY CONFLICT BETWEEN THESE NOTES AND DETAILS AND OTHER PROJECT DRAWINGS.
4. THESE ACCESSIBILITY NOTES AND DETAILS ARE INTENDED TO DETECT SLOPE AND DIMENSIONAL REQUIREMENTS ONLY.

**ACCESSIBLE ROUTE NOTES:**

- 1. AT LEAST ONE ACCESSIBLE ROUTE SHALL BE PROVIDED WITHIN THE SITE FROM ACCESSIBLE PARKING SPACES AND ACCESSIBLE PASSENGER LOADING ZONES.
2. AT LEAST ONE ACCESSIBLE ROUTE SHALL CONNECT ACCESSIBLE BUILDINGS, ACCESSIBLE FACILITIES, ACCESSIBLE ELEMENTS, AND ACCESSIBLE SPACES THAT ARE ON THE SAME SITE.
3. WALKING SURFACES THAT ARE PART OF AN ACCESSIBLE ROUTE SHALL HAVE A MAXIMUM RUNNING SLOPE OF 5.0% AND A MAXIMUM CROSS SLOPE OF 2.0%.
4. ANY WALKING SURFACE THAT IS PART OF AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 5.0% IS A RAMP AND SHALL COMPLY WITH THE GUIDELINES FOR RAMPS OR CURB RAMPS.
5. TRANSITIONS BETWEEN RAMPS, WALKS, LANDINGS, GUTTERS OR STREETS SHALL BE FLUSH/AND FREE OF ABRUPT VERTICAL CHANGES.
6. FLOOR SURFACES SHALL BE STABLE, FIRM AND SLIP RESISTANT.
7. THE MINIMUM CLEAR WIDTH OF EXTERIOR ACCESSIBLE ROUTES SHALL BE FORTY-EIGHT (48) INCHES MINIMUM.
8. WHERE AN ACCESSIBLE ROUTE MAKES A 180 DEGREE TURN AROUND AN OBJECT THAT IS LESS THAN FORTY-EIGHT (48) INCHES IN WIDTH, CLEAR WIDTH SHALL BE FORTY-TWO (42) INCHES MINIMUM.
9. AN ACCESSIBLE ROUTE WITH A CLEAR WIDTH LESS THAN SIXTY (60) INCHES SHALL PROVIDE PASSING SPACES AT INTERVALS OF TWO HUNDRED (200) FEET MAXIMUM.
10. DOORS, DOORWAYS AND GATES THAT ARE PART OF AN ACCESSIBLE ROUTE SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS).
11. DIRECTIONAL SIGNAGE INDICATING THE ROUTE TO THE NEAREST ACCESSIBLE BUILDING ENTRANCE SHALL BE PROVIDED AT INACCESSIBLE BUILDING ENTRANCES.
12. WHERE POSSIBLE, DRAINAGE INLETS SHALL NOT BE LOCATED ON AN ACCESSIBLE ROUTE.

**RAMP NOTES:**

- 1. ANY PART OF AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 5% SHALL BE CONSIDERED A RAMP.
2. THE MAXIMUM RUNNING SLOPE FOR A RAMP SHALL BE 8.33% AND THE MAXIMUM CROSS SLOPE SHALL BE 2.0%.
3. THE CLEAR WIDTH OF AN EXTERIOR RAMP RUN SHALL BE FORTY EIGHT INCHES IN BUILDING CODE 1104.1.
4. THE RISE FOR ANY RAMP RUN SHALL BE THIRTY (30) INCHES MAXIMUM.
5. LANDINGS SHALL BE PROVIDED AT THE TOP AND BOTTOM OF RAMPS.
6. RAMP RUNS WITH A RISE GREATER THAN SIX (6) INCHES SHALL HAVE HANDRAILS ON BOTH SIDES.
7. FLOOR SURFACES OF RAMPS AND LANDINGS SHALL BE STABLE, FIRM AND SLIP RESISTANT.
8. EDGE PROTECTION COMPLYING WITH AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS).
9. WHERE DOORWAYS ARE LOCATED ADJACENT TO A RAMP LANDING, MANEUVERING CLEARANCES REQUIRED BY THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS).
10. CURB RAMP NOTES:

- 1. THE MAXIMUM RUNNING SLOPE OF A CURB RAMP SHALL BE 8.33% AND THE MAXIMUM CROSS SLOPE SHALL BE 2.0%.
2. COUNTER SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE CURB RAMP SHALL NOT BE STEEPER THAN 5%.
3. THE CLEAR WIDTH OF A CURB RAMP SHALL BE 36 INCHES (36) MINIMUM.
4. LANDINGS SHALL BE PROVIDED AT THE TOP OF CURB RAMPS.
5. IF A CURB RAMP IS LOCATED WHERE PEDESTRIANS MUST WALK ACROSS THE RAMP, OR WHERE IT IS NOT PROTECTED BY HANDRAILS OR GUARDRAILS, IT SHALL HAVE FLARED SIDES.
6. WHERE PROVIDED, CURB RAMP FLARES SHALL NOT EXCEED 10%.
7. CURB RAMPS SHALL BE LOCATED OR PROTECTED TO PREVENT THEIR OBSTRUCTION BY PARKED VEHICLES.
8. IT IS RECOMMENDED TO PROVIDE CURB RAMPS WITH A TWENTY-FOUR (24) INCH DEEP DETECTABLE WARNING COMPLYING WITH 406.12 A117.1.
9. CURB RAMPS SHALL BE LOCATED OR PROTECTED TO PREVENT THEIR OBSTRUCTION BY PARKED VEHICLES.
10. FLOOR SURFACES OF CURB RAMPS SHALL BE DEEP GROOVED, 1/2 INCH WIDE BY 1/2 INCH DEEP, ONE (1) INCH CENTERS TRANSVERSE TO THE RAMP.
11. WHERE PROVIDED, STOP LINES SHALL BE LOCATED IN ADVANCE OF CURB RAMP.
12. WHERE PROVIDED, PEDESTRIAN ACTIVATED SIGNALS SHALL BE LOCATED ADJACENT TO THE SIDEWALK AND NOT ON THE SIDEWALK.
13. WHERE PROVIDED, DRAINAGE INLETS SHALL BE LOCATED UPSTREAM OF CURB RAMPS AND NOT IN THE RAMP AREA.
14. CURB RAMP TYPE AND LOCATION ARE PER PLAN.

**PARKING SPACE NOTES:**

- 1. ACCESSIBLE PARKING SPACES SHALL BE LOCATED ON THE SHORTEST ACCESSIBLE ROUTES OF TRAVEL FROM ADJACENT PARKING TO AN ACCESSIBLE BUILDING ENTRANCE.
2. ACCESSIBLE PARKING SPACES SHALL BE AT LEAST NINETY-SIX (96) INCHES WIDE.
3. PARKING SPACES AND ACCESSIBLE SPACES SHALL BE LEVEL WITH SURFACE SLOPES NOT EXCEEDING 2.0% IN ALL DIRECTIONS.
4. PARKED VEHICLE OVERHANGS SHALL NOT REDUCE THE REQUIRED CLEAR WIDTH OF AN ACCESSIBLE ROUTE.
5. PARKING SPACES FOR VANS AND ACCESSIBLE AND VEHICULAR ROUTES SERVING THEM SHALL PROVIDE A VERTICAL CLEARANCE OF NINETY-EIGHT (98) INCHES MINIMUM.
6. EACH ACCESSIBLE PARKING SPACE SHALL BE PROVIDED WITH SIGNAGE DISPLAYING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY.
7. SIGNAGE AT ACCESSIBLE PARKING SPACES REQUIRED BY THE NC BUILDING CODE SECTION 1106.15 SHALL COMPLY WITH THE REQUIREMENTS OF NORTH CAROLINA GENERAL STATUTE 20A-207.27 AND THE NCDOT UNIFORM MANUAL ON TRAFFIC CONTROL DEVICES.
8. ACCESSIBLE PARKING SPACE, ACCESS AISLE STRIPING, AND INTERNATIONAL SYMBOL OF ACCESSIBILITY SHALL BE PAINTED BLUE (OR ANOTHER COLOR THAT CAN BE DISTINGUISHED FROM PAVEMENT).
9. PASSENGER LOADING ZONE NOTES:

- 1. PASSENGER LOADING ZONES SHALL PROVIDE VEHICULAR PULL-UP SPACE NINETY-SIX (96) INCHES WIDE MINIMUM AND TWENTY (20) FEET LONG MINIMUM.
2. PASSENGER LOADING ZONES SHALL PROVIDE A CLEARLY MARKED ACCESS AISLE THAT IS SIXTY (60) INCHES WIDE MINIMUM AND EXTENDS THE FULL LENGTH OF THE VEHICULAR PULL-UP SPACE.
3. ACCESS AISLE SHALL ADJOIN AN ACCESSIBLE ROUTE AND NOT OVERLAP THE VEHICULAR WAY.
4. VEHICLE PULL-UP SPACES AND ACCESS AISLES SERVING THEM SHALL BE LEVEL WITH SURFACE SLOPES NOT EXCEEDING 2.0% IN ALL DIRECTIONS.
5. FLOOR SURFACES OF VEHICLE PULL-UP SPACES AND ACCESS AISLES SERVING THEM SHALL BE STABLE, FIRM AND SLIP RESISTANT.
6. VEHICLE PULL-UP SPACES, ACCESS AISLES SERVING THEM AND A VEHICULAR ROUTE FROM AN ENTRANCE TO THE PASSENGER LOADING ZONE, AND FROM THE PASSENGER LOADING ZONE TO A VEHICULAR EXIT SERVING THEM, SHALL PROVIDE A VERTICAL CLEARANCE OF ONE HUNDRED FOURTEEN (114) INCHES MINIMUM.

- 1. ACCESSIBLE ENTRANCE NOTES:
2. ENTRANCE DOORS, DOORWAYS AND GATES SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (2010 ADA STANDARDS) THE NC BUILDING CODE/ANSI A117.1 AND SHALL BE ON AN ACCESSIBLE ROUTE.

**GENERAL UTILITY NOTES:**

- 1. ALL STORM SEWERS SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF WILMINGTON AND NCDOT REQUIREMENTS AS SPECIFIED ON THE DRAWINGS AND IN THE PROJECT SPECIFICATIONS.
2. BEDDING FOR ALL STORM SEWER PIPE SHALL BE AS SPECIFIED ON THE DRAWINGS.
3. ALL STORM SEWER PIPES SHOWN AS RCP ON THE PLANS SHALL BE REINFORCED CONCRETE PIPE CONFORMING TO ASTM C-76, UNLESS INDICATED OTHERWISE ON PLANS.

**STORM WATER NOTES:**

- 1. TOTAL SITE AREA = 654,480 SF (15.03 AC)
2. DISTURBED AREA = 609,840 SF (14.00 AC)
3. IMPERVIOUS AREA DRAINING TO STORMWATER SYSTEM = 440,000 SF (10.10 AC)

**WETLAND NOTES:**

- 1) NO WETLANDS EXIST ON-SITE BASED ON A SITE EVALUATION BY ECS CAROLINAS, LLP.

**ROOF DRAIN NOTE:**

- 1) PROPOSED BUILDING SHALL DIVERT ROOF DRAINAGE TO STORMWATER COLLECTION SYSTEM.

**EXISTING UTILITY NOTES:**

- 1. IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO VERIFY THE ACTUAL LOCATION AND AVAILABILITY OF ALL EXISTING AND PROPOSED UTILITIES IN THE FIELD PRIOR TO GROUND BREAKING.
2. EXISTING UTILITIES AND STRUCTURES SHOWN, BOTH UNDERGROUND AND ABOVE GROUND, ARE BASED ON A FIELD SURVEY AND THE BEST AVAILABLE RECORD DRAWINGS.

**LEGEND**

Legend table listing symbols for wetlands, property lines, contours, iron pipe/rod found, concrete monument, telephone pedestal, underground telephone, transformer pedestal, drop inlet, curb sewer, storm drain manhole, drainage inlets, spot elevations, edge of pavement, sidewalk elevation, gutter flow line, curb elevation, contours, drainage areas, silt fence, limits of disturbance, tree protection fencing, inlet protection, traffic arrow, signs, sanitary sewer service, sanitary sewer cleanout, building, and concrete curb & gutter.

**NOT RELEASED FOR CONSTRUCTION**

LATEST DATE: DA16-026/ PC16-064

DRAWN BY: RPB

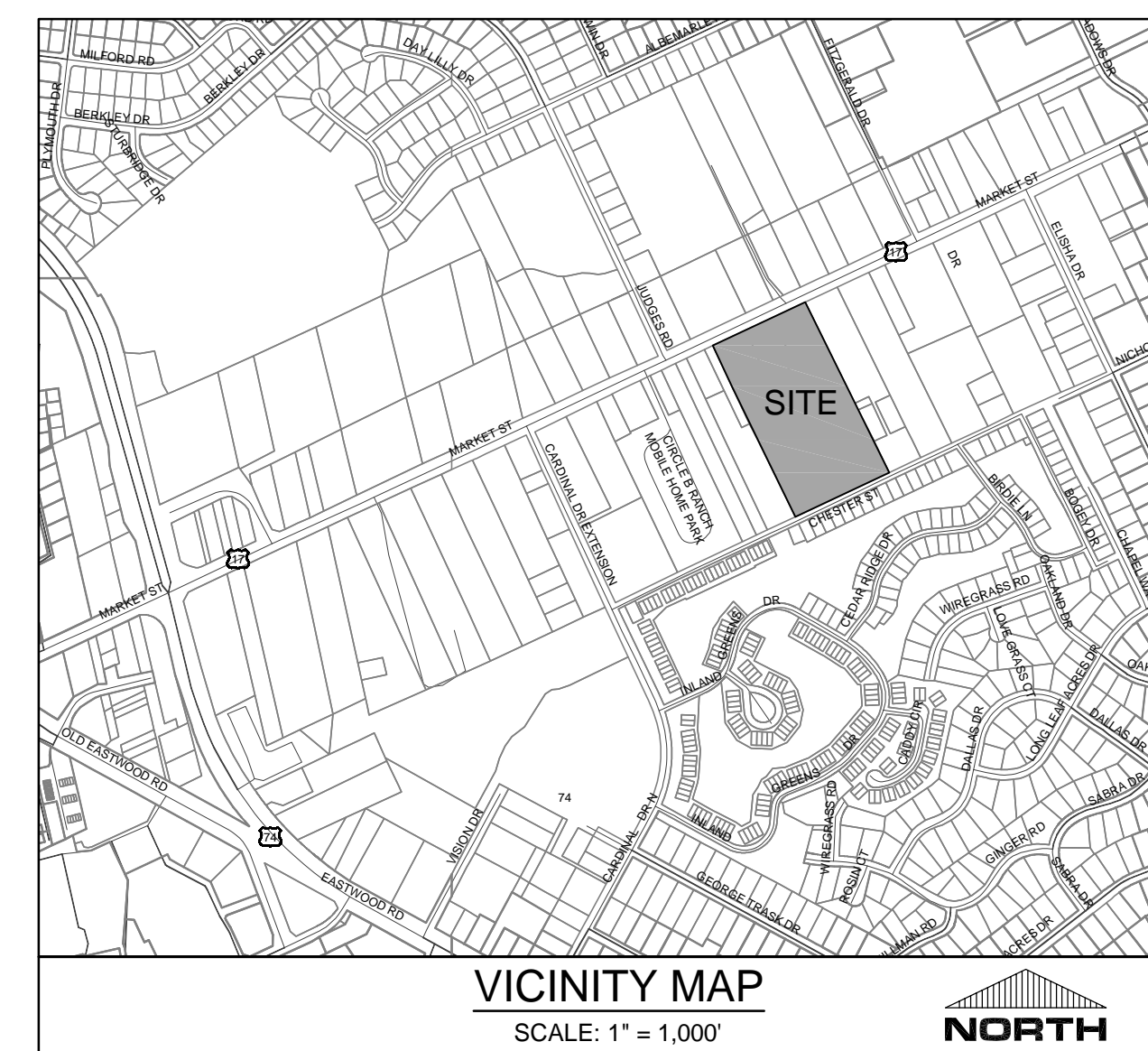
CHECKED BY: RPB

Revisions table with columns: REV#, DATE, DESCRIPTION, BY. Includes one revision dated 09 MAR 17.

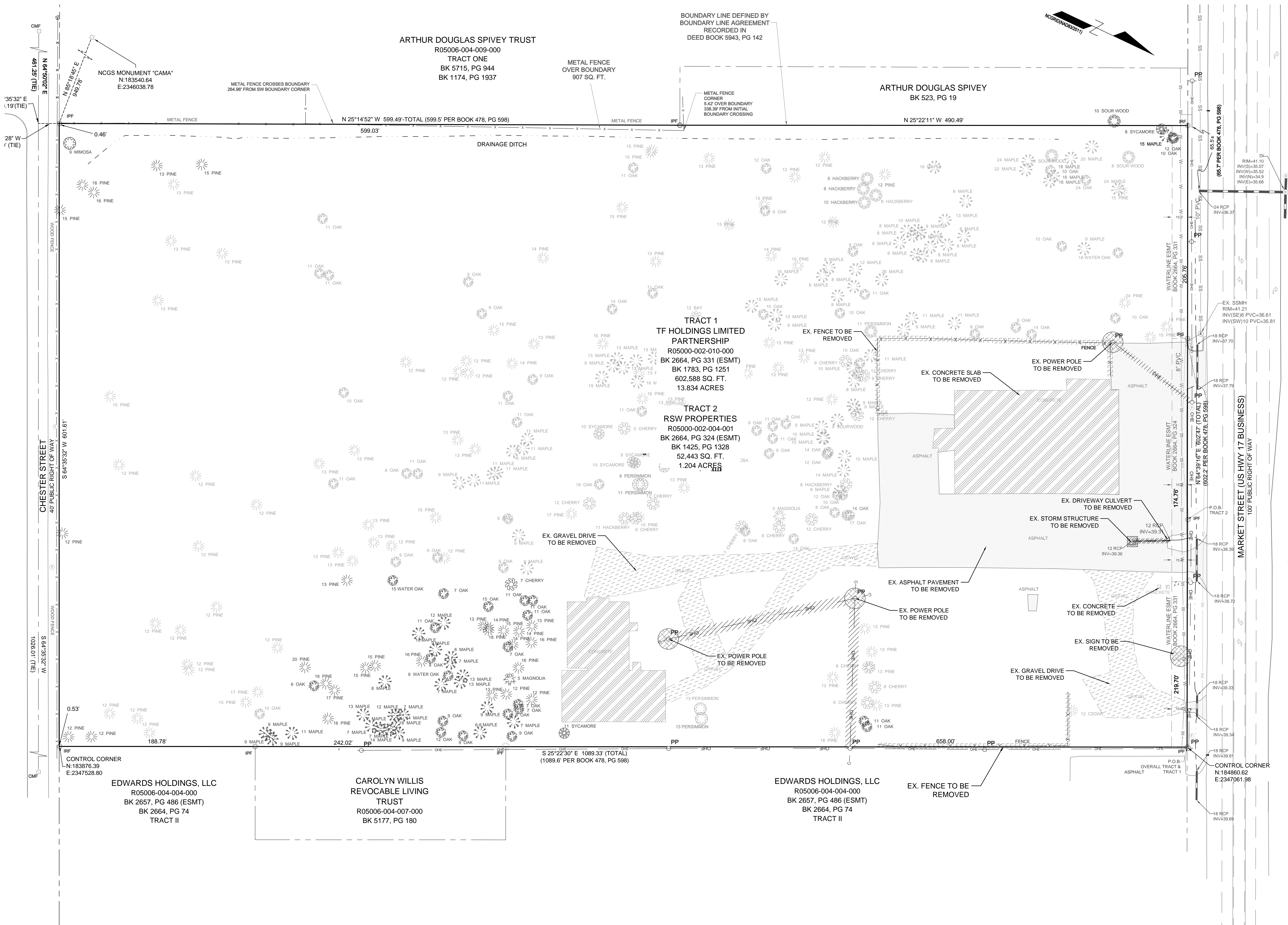
Project information including Project No. 20-15081.02, Date 11 NOV 16, Sheet Title GENERAL NOTES, and Sheet No. C-1.0. Includes logos for Carmax and 811.

NPDES WATER QUALITY STABILIZATION TIME FRAMES table with columns: SITE AREA DESCRIPTION, STABILIZATION, TIMEFRAME EXCEPTIONS.

City of Wilmington logo and Public Services/Engineering Division information. Includes a form for Approved Construction Plan with fields for Name, Date, Planning, Traffic, and Fire.



VICINITY MAP  
SCALE: 1" = 1,000'



- DEMOLITION NOTES:**
- 1) CONTRACTOR SHALL REFER TO SHEET C-1.0 GENERAL NOTES FOR DEMOLITION NOTES.
  - 2) CONTRACTOR SHALL REFER TO THE LANDSCAPE PLANS FOR SPECIFIC TREE REMOVAL.
  - 3) CONTRACTOR SHALL MAKE NO PAVEMENT CUTS INTO MARKET STREET WITHOUT THE WRITTEN PERMISSION OF NCDOT.
  - 4) ALL UTILITIES SHALL BE ABANDONED AND/OR DEMOLISHED IN ACCORDANCE WITH THE UTILITY OWNERS RULES AND REGULATIONS.
  - 5) CONTRACTOR SHALL COORDINATE WITH ELECTRIC UTILITY OWNER/PROVIDER FOR DEMOLITION.

SYMBOLS LEGEND	
REFER TO SURVEY FOR EXISTING CONDITION SYMBOLS LEGEND	
	EXISTING ASPHALT TO BE REMOVED
	EXISTING CONC. CURB TO BE REMOVED
	EXISTING CONCRETE TO BE REMOVED
	EXISTING GRAVEL TO BE REMOVED
	EXISTING UTILITY LINE TO BE REMOVED
	EXISTING STORM SEWER LINE TO BE REMOVED
	EXISTING STORM STRUCTURE TO BE REMOVED
	EXISTING SIGNLIGHT/UTILITY POLE TO BE REMOVED
	EXISTING FENCE TO BE REMOVED
	TREE PROTECTION FENCING

**NOT RELEASED FOR CONSTRUCTION**

**LATEST DATE:** DA16-026/ PC16-064

**DRAWN BY:** RPB

**CHECKED BY:** RPB

REV#	DATE	DESCRIPTION	BY
09	MAR 17	REVISION #1	

**PARAMOUNT ENGINEERING**  
122 Cinema Drive  
Wilmington, North Carolina 28403  
(910) 791-6707 (O) (910) 791-6700 (F)  
NC License # C-2846

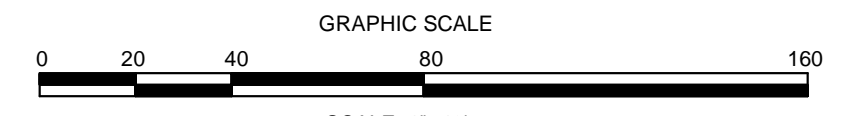
**CITY OF WILMINGTON**  
NORTH CAROLINA  
Public Services • Engineering Division  
APPROVED STORMWATER MANAGEMENT PLAN  
Date: \_\_\_\_\_ Permit # \_\_\_\_\_  
Signed: \_\_\_\_\_

**Approved Construction Plan**

Name	Date
Planning _____	_____
Traffic _____	_____
Fire _____	_____

For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance.

NCDENR PWSS WATER PERMIT #: \_\_\_\_\_ GPD  
WATER CAPACITY: \_\_\_\_\_  
DWQ SEWER PERMIT #: \_\_\_\_\_ GPD  
SEWER CAPACITY: \_\_\_\_\_  
SEWER SHED # AND PLANT: \_\_\_\_\_  
SEWER TO FLOW THROUGH NEI: YES or NO (CIRCLE ONE)

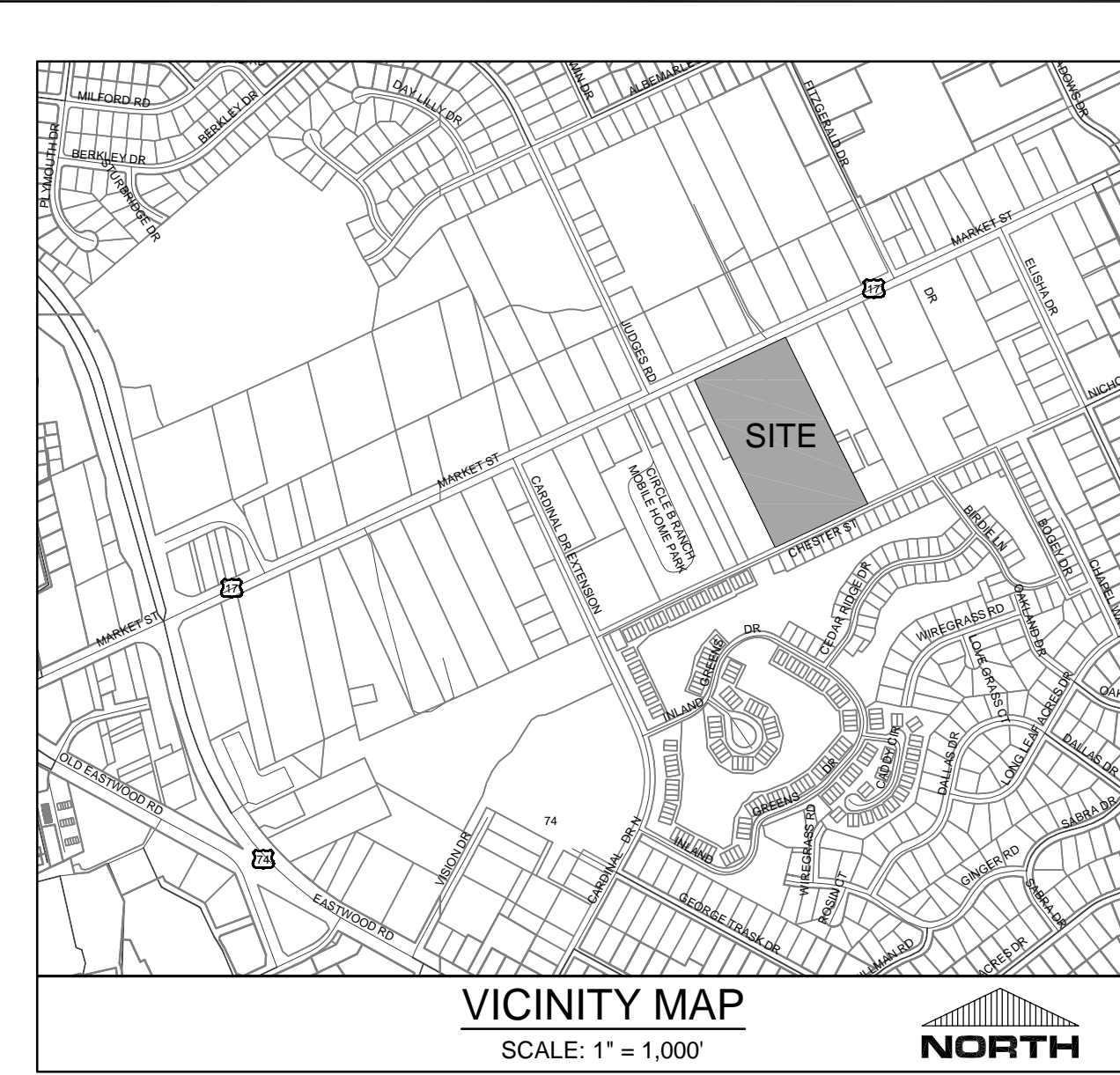


**CARMAX**  
THE AUTO SUPERSTORE  
CARMAX FINANCIAL SERVICES, INC.  
1801 WYCKOFF DRIVE, WILMINGTON, NC 28403

**STORE NUMBER 6022**  
**6030 MARKET STREET**  
**WILMINGTON, NC 28405**

**PROJECT NO.:** 20-15081.02  
**DATE:** 11 NOV 16  
**SHEET TITLE:** DEMOLITION PLAN

**SHEET NO.:** C-2.0



**SITE DATA TABULATION**

OWNER: CARMAX OF WILMINGTON  
 PROJECT ADDRESS: 6030 MARKET STREET WILMINGTON, NC 28403  
 TAX PARCEL IDENTIFICATION #: R05000-002-010-000 (13.83 AC) - LOT 1 R05000-002-004-001 (1.20 AC) - LOT 2 BK 2664, PG 331 (ESMT) - LOT 1 BK 1783, PG 1251 - LOT 1 BK 2664, PG 324 (ESMT) - LOT 2 BK 1425, PG 1328 - LOT 2 15.038 ACRES (655,055 SF)  
 RECORDED DEED BOOK: RB - REGIONAL BUSINESS URBAN  
 CURRENT ZONING: UNDEVELOPED  
 CAMA LAND USE CLASSIFICATION: AUTOMOBILE DEALER - USED  
 MAXIMUM ALLOWABLE BUILDING HEIGHT: 35'  
 EXISTING LAND USE: UNDEVELOPED  
 PROPOSED LAND USE: AUTOMOBILE DEALER - USED

**SETBACK STANDARDS**

MINIMUM FRONT SETBACK	PROVIDED	REQUIRED
141.3 FT. (NORTH)	141.3 FT.	25 FT.
MINIMUM SIDE SETBACK	254.6 FT. (EAST)	0 FT.
MINIMUM SIDE SETBACK	97 FT. (WEST)	0 FT.
MINIMUM REAR SETBACK	661 FT. (SOUTH)	15 FT.

**SITE INFORMATION**

LOT AREA	654,480 SF (15.03 AC)	43,560 SF (1 AC)
LOT WIDTH	602.47'	100' MIN.
LOT COVERAGE	6.29%	40% MAX.
PROPOSED IMPERVIOUS AREA	440,000 / 654,480 SF = 67.2%	
TOTAL BUILDING (PROPOSED FOOTPRINT)	41,254 SF	
ON-SITE PARKING & DRIVEWAYS	384,900 SF	
GRAVEL DRIVE	4,350 SF	
ON-SITE SIDEWALKS	4,900 SF	
FUTURE IMPERVIOUS	4,596 SF	
TOTAL PROPOSED IMP. AREA	440,000 SF (10.10 AC)	

**BUILDING INFORMATION**

CONSTRUCTION TYPE	1B
NUMBER OF PROPOSED BUILDINGS	1
PROPOSED BUILDING SF	41,254 SF
OVERALL HEIGHT	28'-0"
LOT COVERAGE	5.93% (PROPOSED)

**SITE PARKING**

MINIMUM PARKING REQUIRED: (1 PER 500 SF GFA)	83
MAXIMUM PARKING ALLOWABLE:	125 SPACES
TOTAL PARKING PROVIDED:	215 CUSTOMER PARKING SPACES 224 SALES DISPLAY AREA SPACES

**HANDICAP PARKING REQUIRED:** 9 SPACES  
**HANDICAP PARKING PROVIDED:** 9 SPACES

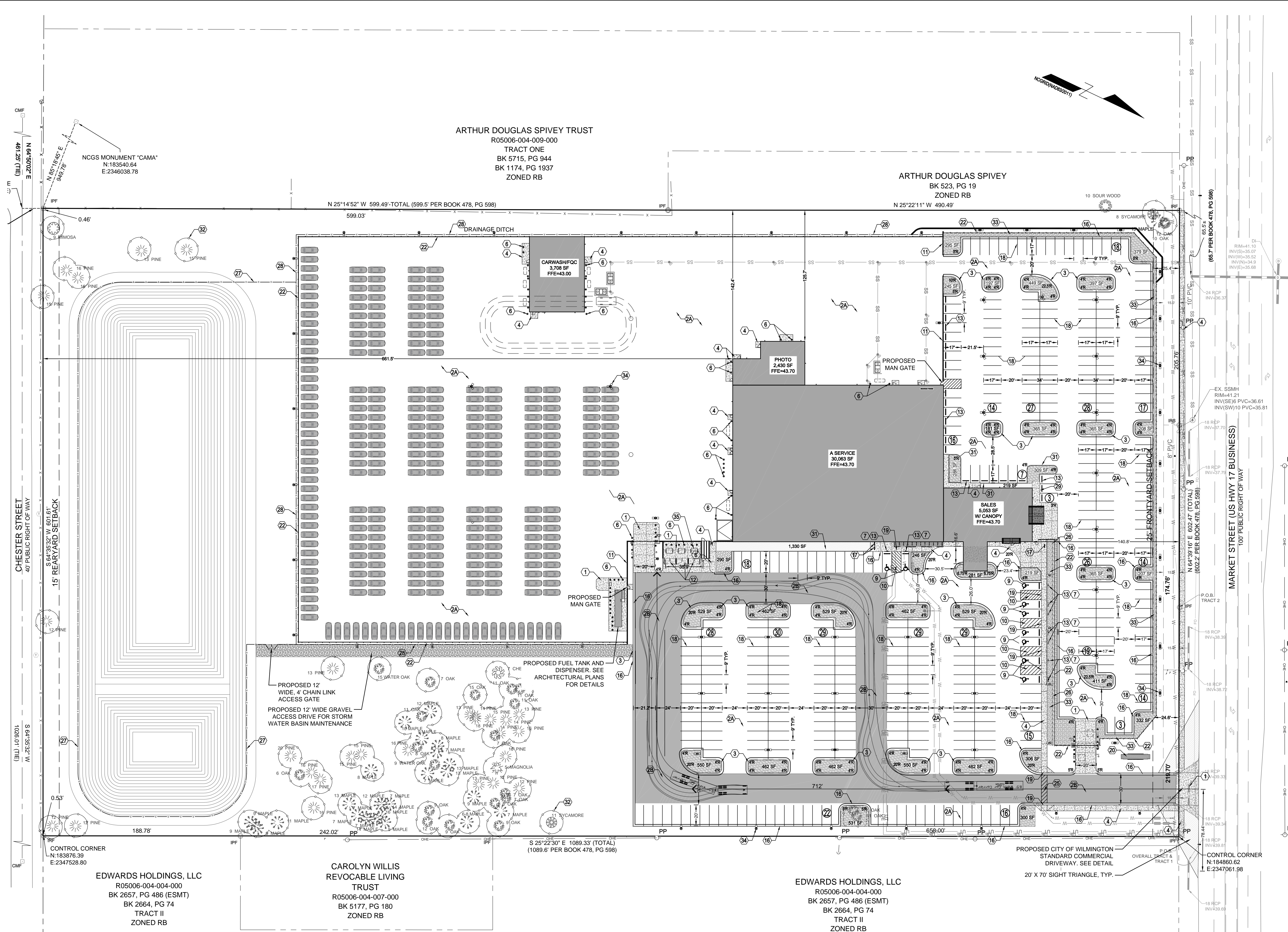
**BICYCLE PARKING REQUIRED:** 15 SPACES  
**BICYCLE PARKING PROVIDED:** 15 SPACES

**FLOOD NOTE**  
 THE SUBJECT PARCEL IS NOT LOCATED WITHIN A 100-YEAR FLOOD ZONE AS INDICATED BY FEMA FLOOD ZONE PANEL 3720314800I BEARING AN EFFECTIVE DATE OF APRIL 3, 2006.

- GENERAL NOTES**
- ALL PAVEMENT MARKINGS IN PUBLIC RIGHTS-OF-WAY AND FOR DRIVEWAYS ARE TO BE THERMOPLASTIC AND MEET CITY AND/OR NCDOT STANDARDS.
  - ALL SIGNS AND PAVEMENT MARKINGS IN AREAS OPEN TO PUBLIC TRAFFIC ARE TO MEET MUTCD STANDARDS.
  - ALL TRAFFIC CONTROL SIGNS AND MARKINGS OFF THE RIGHT OF WAY ARE TO BE MAINTAINED BY THE PROPERTY OWNER IN ACCORDANCE WITH MUTCD STANDARDS.
  - ALL PARKING STALL MARKINGS AND LANE ARROWS WITHIN THE PARKING AREAS SHALL BE WHITE.
  - ANY BROKEN OR MISSING SIDEWALK PANELS, DRIVEWAY PANELS AND CURBING WILL BE REPLACED.

**VARIANCE REQUESTS:**

- DRIVE ISLE WIDTH**  
 WE WOULD LIKE TO REQUEST A VARIANCE FOR THE SALES DISPLAY AREA BE APPROVED WITH 20' DRIVE AREA AND NOT THE REQUIRED 24'. THE GENERAL PUBLIC IS NOT ALLOWED TO DRIVE IN THE SALES ISLE, ONLY CARMAX EMPLOYEES, AND IT MEETS THE MINIMUM WIDTH FOR FIRE SERVICES. ALL OTHER DRIVE ISLES OUTSIDE THE SALES AREA WILL MEET THE 24' MINIMUM.
- EXCEEDING MAXIMUM PARKING**  
 WE WOULD LIKE TO REQUEST A VARIANCE FOR EXCEEDING THE MAXIMUM PARKING SPACES. THE PLAN PROPOSES 215 PARKING SPACES FOR CUSTOMER/EMPLOYEE PARKING, THE MAXIMUM PER CODE IS 125 SPACES. THE PLAN ALSO HAS 224 DISPLAY SPACES IN THE SALES AREA, WHICH ARE SECURED AND GATED WITH NO ACCESS ALLOWED BY THE GENERAL PUBLIC. THE ADDITIONAL PARKING SPACES FOR CUSTOMER/EMPLOYEE PARKING AREA IS FOR THE MONTHLY WHOLESALE AUCTION THAT IS HELD FOR SELLING VEHICLES THAT HAVE BEEN TRADED AND NOT UP TO CARMAX'S STANDARDS.
- PRIVATE DRIVEWAYS**  
 WE WOULD LIKE TO REQUEST A VARIANCE FOR EXCEEDING THE 500' ALLOWED FOR A PRIVATE DRIVEWAY ON THE BASIS THAT THE PROPOSED DRIVEWAY AND PARKING CONFIGURATION DOES NOT PROVIDE UNSAFE PARKING, MANEUVERING OR ACCESS FOR THE GENERAL PUBLIC USER.



**KEY NOTES:**

- |   |   |  |
|---|---|--|
| 1 CONCRETE PAVING: REFER TO DETAIL C-6.0                  | 9 HANDICAP PARKING SYMBOL: REFER TO DETAIL C-6.0  | 17 BICYCLE RACK (5 BIKE MIN. PER RACK): CONTRACTOR SHALL SUBMIT SHOP DWG. FOR REVIEW                     |
| 2A LIGHT DUTY ASPHALT PAVING: REFER TO DETAIL C-6.0       | 10 HANDICAP ACCESS UNLOADING ZONE: SLOPE 2% MAX. EACH WAY (ADA COMPLIANT) AND STRIPE AS SHOWN.  | 18 STRIPING: PROVIDE 4' WIDE PARKING LOT STRIPING AS SHOWN. USE HIGHWAY MARKING PAINT - WHITE (2 COATS). |
| 2B HEAVY DUTY ASPHALT PAVING: REFER TO DETAIL C-6.0       | 11 SCREEN WALL: REFER TO DETAIL C-6.2   | 19 DETECTIBLE WARNING MAT: REFER TO DETAIL C-6.0   |
| 3 MOUNTABLE CONCRETE CURB & GUTTER: REFER TO DETAIL C-6.0 | 12 SCREEN WALL GATES: REFER TO DETAIL C-6.2   | 20 SIGN LOCATION: SIGN FURNISHED AND INSTALLED BY OWNER. REFER TO ARCHITECTURAL PLANS FOR LOCATION       |
| 4 CONCRETE SIDEWALK: REFER TO DETAIL C-6.0                | 13 CONCRETE BUMPER BLOCK: 8" x 5" x 6'-0" LONG CONCRETE. ANCHOR TO PAVING WITH (2) 1"-6" LONG #4 REBAR. REFER TO DETAIL C-6.0         | 21 CONCRETE DRIVE APRON: NOT APPLICABLE  |
| 5 CONCRETE DOOR LANDING: NOT APPLICABLE                   | 14 PARKING LOT LIGHTING: REFER TO SITE LIGHTING PLAN FOR LOCATION AND TYPE.   | 22 METAL GUARD RAIL: REFER TO DETAIL C-6.0   |
| 6 STEEL BOLLARD: REFER TO DETAIL C-6.0                    | 15 LIMITS OF NEW PAVING: IN NEW PARKING AND DRIVE AREA NOT DESIGNATED AS HEAVY DUTY STANDARD 24" CURB & GUTTER: REFER TO DETAIL C-6.0 | 23 CONCRETE FLUME: NOT APPLICABLE  |
| 7 HANDICAP PARKING SIGN: REFER TO DETAIL C-6.0            | 16 CONCRETE SIDEWALK: PER CITY/STATE DESIGN STANDARDS. IF NO STANDARDS, REFER TO DETAIL C-6.0   | 24 RETAINING WALL: NOT APPLICABLE  |

- |   |  |
|---|--|
| 25 DESIGNATED ACCESSIBLE ROUTE: PROVIDE STRIPING AND SLOPE 5% MAXIMUM IN DIRECTION OF TRAVEL WITH 2% MAXIMUM CROSS SLOPE. | 33 CLASS A RIP-RAP STONE: IN ACCORDANCE WITH NCDOT STANDARD SPECIFICATIONS MANUAL, SECTION 1043. |
| 26 ORNAMENTAL FENCING: REFER TO DETAIL C-6.0  | 34 SITE LIGHTING: SEE SITE PHOTOMETRIC PLANS FOR POLE AND FOUNDATION DESIGN.                     |
| 27 4' CHAINLINK FENCE (BLACK): SEE DETAIL C-X-X   | 35 PROPOSED DUMPSTERS: REFER TO DETAIL C-6.9   |
| 28 6' CHAINLINK FENCE (BLACK) W/ VISION SLATS: SEE DETAIL C-X-X   | 36 CONCRETE SIDEWALK DETAIL: 6.5" EXPOSED FACE TO ASPHALT SEE DETAIL C-6.0                       |
| 29 CONCRETE DRIVE APRON: NOT APPLICABLE   | 37 EMERGENCY VEHICLE LANE MARKING: NOT APPLICABLE  |
| 30 CONCRETE DRIVE APRON: NOT APPLICABLE   | 38 HEADER CURB: REFER TO DETAIL C-6.0  |
| 31 TREE PROTECTION FENCING: SEE TREE REMOVAL PLAN C-2.3   |  |

**CITY OF WILMINGTON**  
 NORTH CAROLINA  
 Public Services • Engineering Division  
 APPROVED STORMWATER MANAGEMENT PLAN

Date: \_\_\_\_\_ Permit # \_\_\_\_\_  
 Signed: \_\_\_\_\_

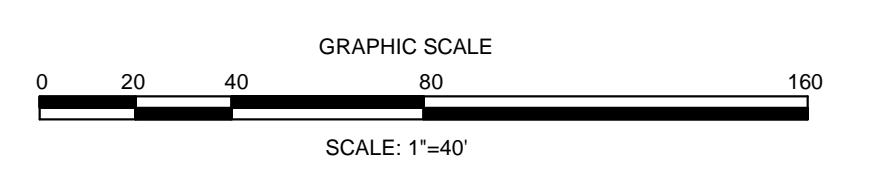
**Approved Construction Plan**

Name \_\_\_\_\_ Date \_\_\_\_\_

Planning \_\_\_\_\_  
 Traffic \_\_\_\_\_  
 Fire \_\_\_\_\_

For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance.

NC DENR PWSS WATER PERMIT #: \_\_\_\_\_  
 WATER CAPACITY: \_\_\_\_\_ GPD  
 DWQ SEWER PERMIT #: \_\_\_\_\_  
 SEWER CAPACITY: \_\_\_\_\_ GPD  
 SEWER SHED # AND PLANT: \_\_\_\_\_  
 SEWER TO FLOW THROUGH NEI: YES or NO (CIRCLE ONE)



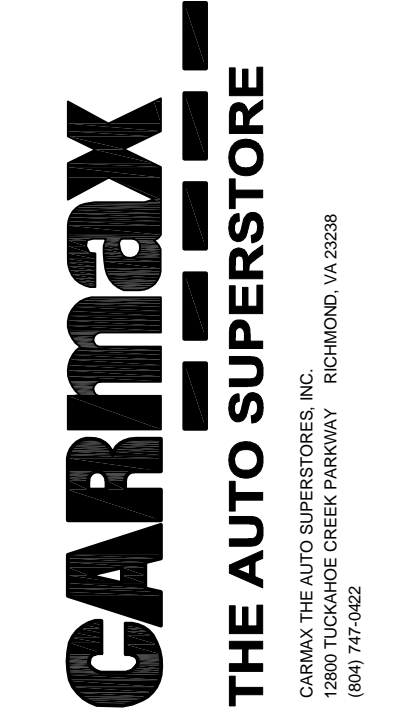
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**LATEST DATE:** DA16-026/ PC16-064

**DRAWN BY:** RPB

**CHECKED BY:** RPB

REVISIONS	REV#	DATE	DESCRIPTION	BY
	09	MAR 17	REVISION #1	



**STORE NUMBER 6022**  
**6030 MARKET STREET**  
**WILMINGTON, NC 28405**

**PROJECT NO.:** 20-15081.02  
**DATE:** 11 NOV 16  
**SHEET TITLE:** SITE PLAN  
**SHEET NO.:** C-2.1

**PARAMOUNT ENGINEERING**  
 122 Cinema Drive  
 Wilmington, North Carolina, 28403  
 (910) 791-6707 (F) (910) 791-6700 (F)  
 NC License #: C-2846

**NOT RELEASED FOR CONSTRUCTION**

**LATEST DATE:** DA16-026/PC16-064

**DRAWN BY:** RPB

**CHECKED BY:** RPB

REV#	DATE	DESCRIPTION	BY
09	MAR 17	REVISION #1	

**CARMAX**  
 THE AUTO SUPERSTORE  
CARMAX AUTO SUPERSTORES, INC. A CREDITORS COMPANY. ESTABLISHED IN 1978. FINANCED BY CREDITORS.

**STORE NUMBER 6022**  
**6030 MARKET STREET**  
**WILMINGTON, NC 28405**

**811**  
 Know what's below.  
 Call before you dig.

<b>PROJECT NO.</b>	20-15081.02
<b>DATE</b>	11 NOV 16
<b>SHEET TITLE</b>	SITE INVENTORY MAP
<b>SHEET NO.</b>	C-2.2

For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance.

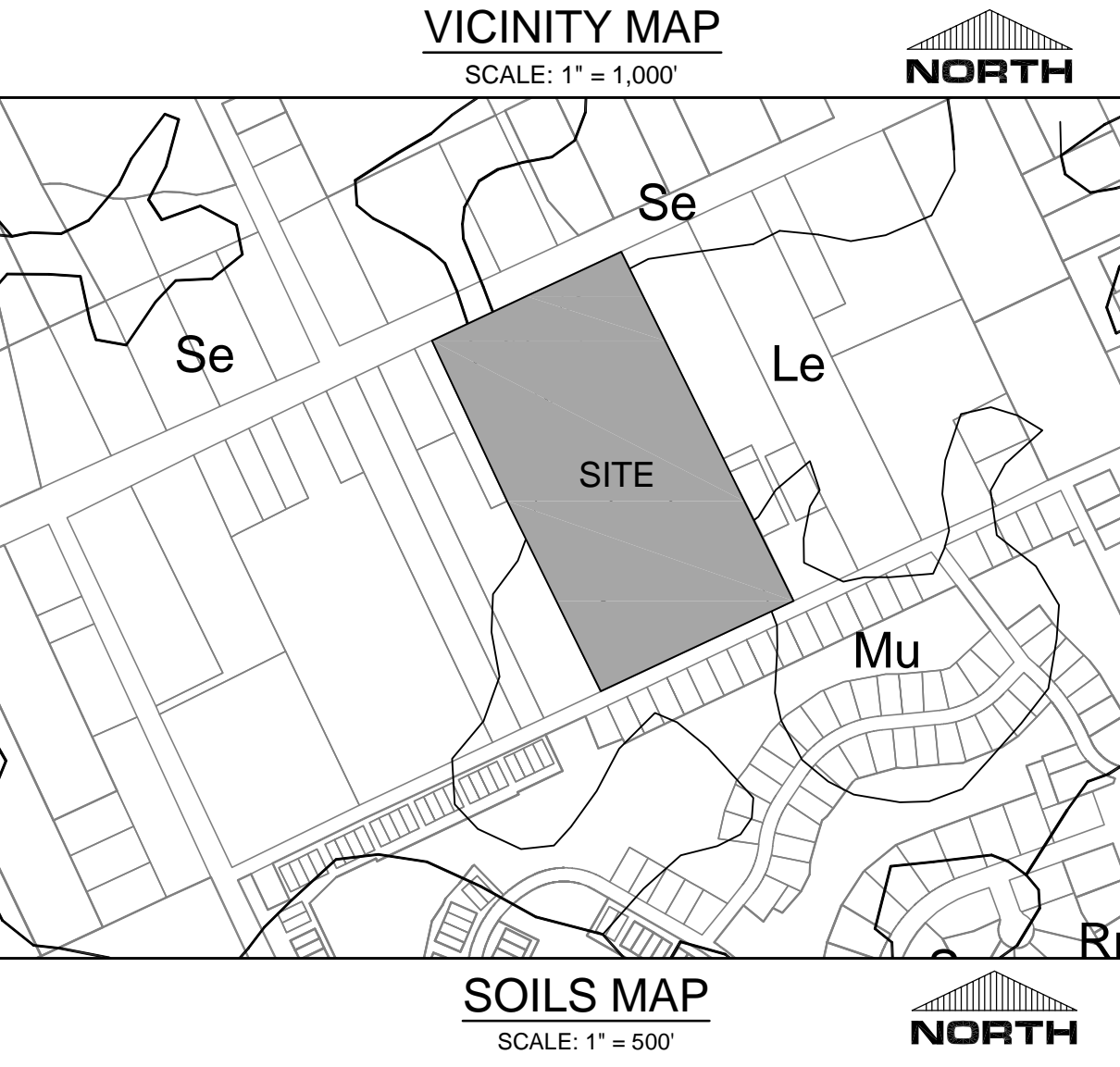
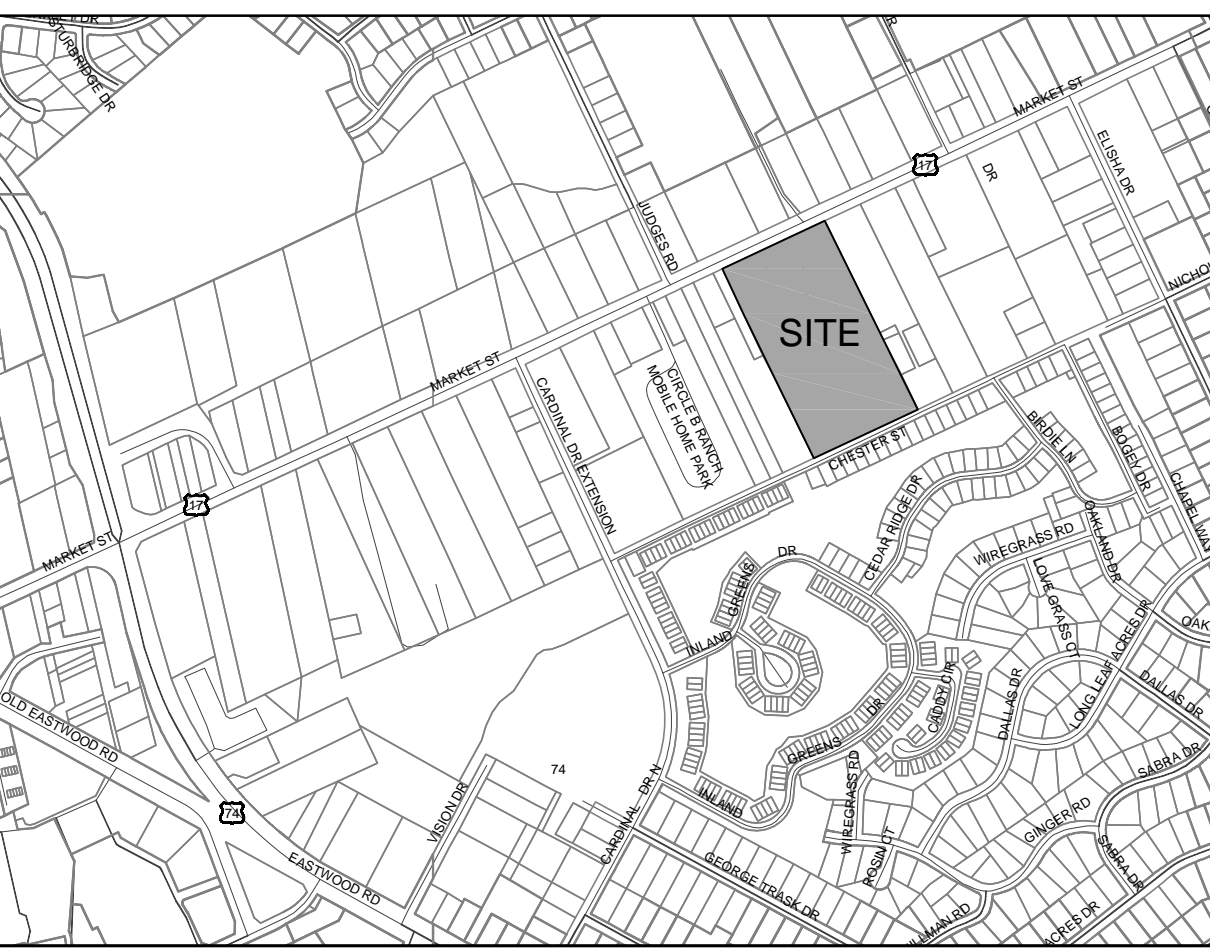
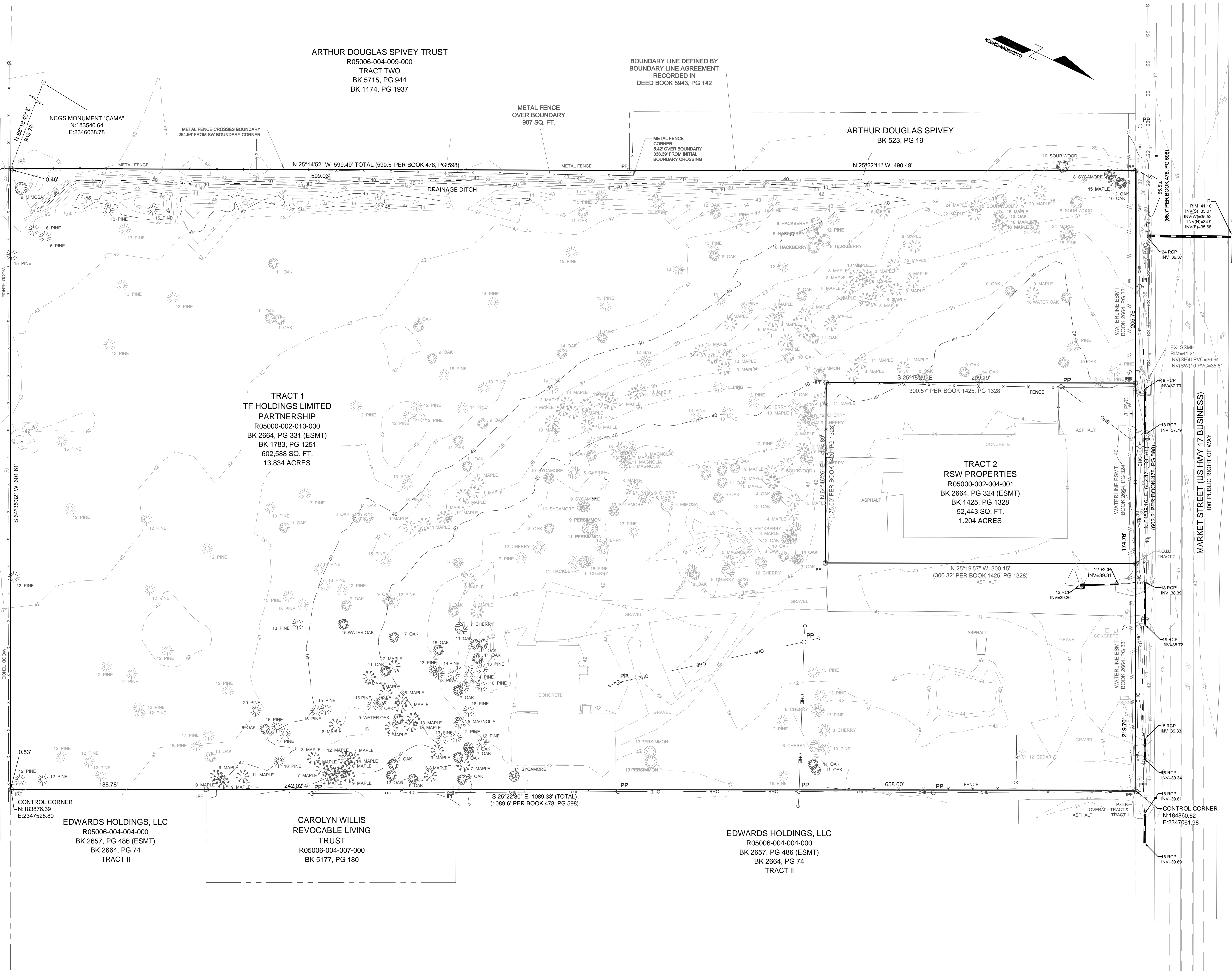
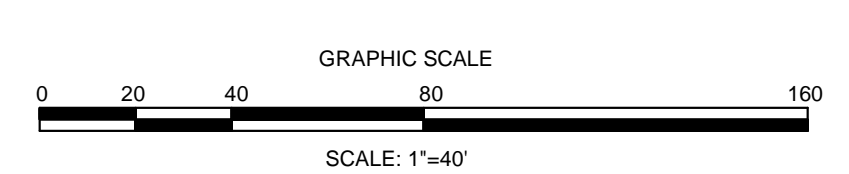
NCDENR PWSS WATER PERMIT #: \_\_\_\_\_  
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 SEWER CAPACITY: \_\_\_\_\_ GPD  
 SEWER SHED # AND PLANT: \_\_\_\_\_  
 SEWER TO FLOW THROUGH NEI: YES or NO (CIRCLE ONE)

**CITY OF WILMINGTON**  
 NORTH CAROLINA  
 Public Services • Engineering Division  
 APPROVED STORMWATER MANAGEMENT PLAN

Date: \_\_\_\_\_ Permit # \_\_\_\_\_  
 Signed: \_\_\_\_\_

**Approved Construction Plan**

Name	Date
Planning _____	_____
Traffic _____	_____
Fire _____	_____



**SITE INVENTORY DATA**

- PREPARER OF THE PLAN: PARAMOUNT ENGINEERING, 122 CINEMA DRIVE, WILMINGTON, NC 28403
- APPLICANT NAME: CARMAX AUTO SUPERSTORES, INC., 12800 TUCKAHOE CREEK PARKWAY, RICHMOND, VA 23238
- SITE ADDRESS: 6030 MARKET STREET, WILMINGTON, NC 28403
- PROPERTY OWNER: TF HOLDINGS LIMITED PARTNERSHIP (TRACT 1), RSW PROPERTIES (TRACT 2)
- DEVELOPER: SAME AS APPLICANT
- PROPERTY BOUNDARY: SEE PLAN
- ZONING: RB - REGIONAL BUSINESS
- ADJACENT PROPERTY INFORMATION (OWNER AND ZONING): SEE PLAN
- VICINITY MAP: SEE MAP ABOVE
- TOPOGRAPHY: SEE PLAN
- 100 YEAR FLOOD PLAIN BOUNDARY: SITE IS NOT LOCATED IN THE 100 YEAR FLOOD PLAIN
- LOCATION OF EXISTING DITCHES, CREEKS, AND STREAMS: SEE PLAN
- SOILS: MURVILLE (Mu), LEON (Le), SEAGATE (Se) (SEE SOILS MAP ABOVE)
- CAMA AEC & ASSOCIATED SETBACKS: N/A
- CAMA LAND CLASSIFICATION: URBAN
- CONSERVATION RESOURCES: N/A
- LOCAL, STATE, OR FEDERAL HISTORIC OR ARCHAEOLOGICAL SITE: N/A
- LOCATION OF CEMETERIES, BURIAL SITES, OR BURIAL GROUNDS: N/A
- SQUARE FOOTAGE OF FORESTED AREA, HABITAT, AND DOMINANT SPECIES: N/A
- SECTION 404 WETLANDS AND SECTION 10 WATERS: N/A
- PROTECTED SPECIES OR HABITAT: N/A
- THOROUGHFARES, BIKE ROUTES, SIDEWALKS, TRANSIT FACILITIES (EXISTING OR PROPOSED): SEE PLAN & VICINITY MAP

**ARTHUR DOUGLAS SPIVEY TRUST**  
 R05006-004-009-000  
 TRACT TWO  
 BK 5715, PG 944  
 BK 1174, PG 1937

**ARTHUR DOUGLAS SPIVEY**  
 BK 523, PG 19

**TRACT 1**  
**TF HOLDINGS LIMITED PARTNERSHIP**  
 R05000-002-010-000  
 BK 2664, PG 331 (ESMT)  
 BK 1783, PG 1251  
 602,588 SQ. FT.  
 13.834 ACRES

**TRACT 2**  
**RSW PROPERTIES**  
 R05000-002-004-001  
 BK 2664, PG 324 (ESMT)  
 BK 1425, PG 1328  
 52,443 SQ. FT.  
 1.204 ACRES

**EDWARDS HOLDINGS, LLC**  
 R05006-004-004-000  
 BK 2657, PG 486 (ESMT)  
 BK 2664, PG 74  
 TRACT II

**CAROLYN WILLIS REVOCABLE LIVING TRUST**  
 R05006-004-007-000  
 BK 5177, PG 180

**EDWARDS HOLDINGS, LLC**  
 R05006-004-004-000  
 BK 2657, PG 486 (ESMT)  
 BK 2664, PG 74  
 TRACT II

OWNER: CARMAX OF WILMINGTON  
 PROJECT ADDRESS: 6030 MARKET STREET  
 WILMINGTON, NC 28403  
 TAX PARCEL IDENTIFICATION #: R05000-002-010-000 (13.83 AC) - LOT 1  
 R05000-002-004-001 (1.20 AC) - LOT 2  
 BK 2664, PG 331 (ESMT) - LOT 1  
 BK 1783, PG 1251 - LOT 1  
 BK 2664, PG 324 (ESMT) - LOT 2  
 BK 1425, PG 1328 - LOT 2  
 15.038 ACRES (655,055 SF)  
 RB - REGIONAL BUSINESS URBAN  
 CURRENT ZONING: 35  
 CAMA LAND USE CLASSIFICATION: UNDEVELOPED  
 MAXIMUM ALLOWABLE BUILDING HEIGHT: UNDEVELOPED  
 EXISTING LAND USE: UNDEVELOPED  
 PROPOSED LAND USE: AUTOMOBILE DEALER - USED

**6030 Market St Tree Survey**

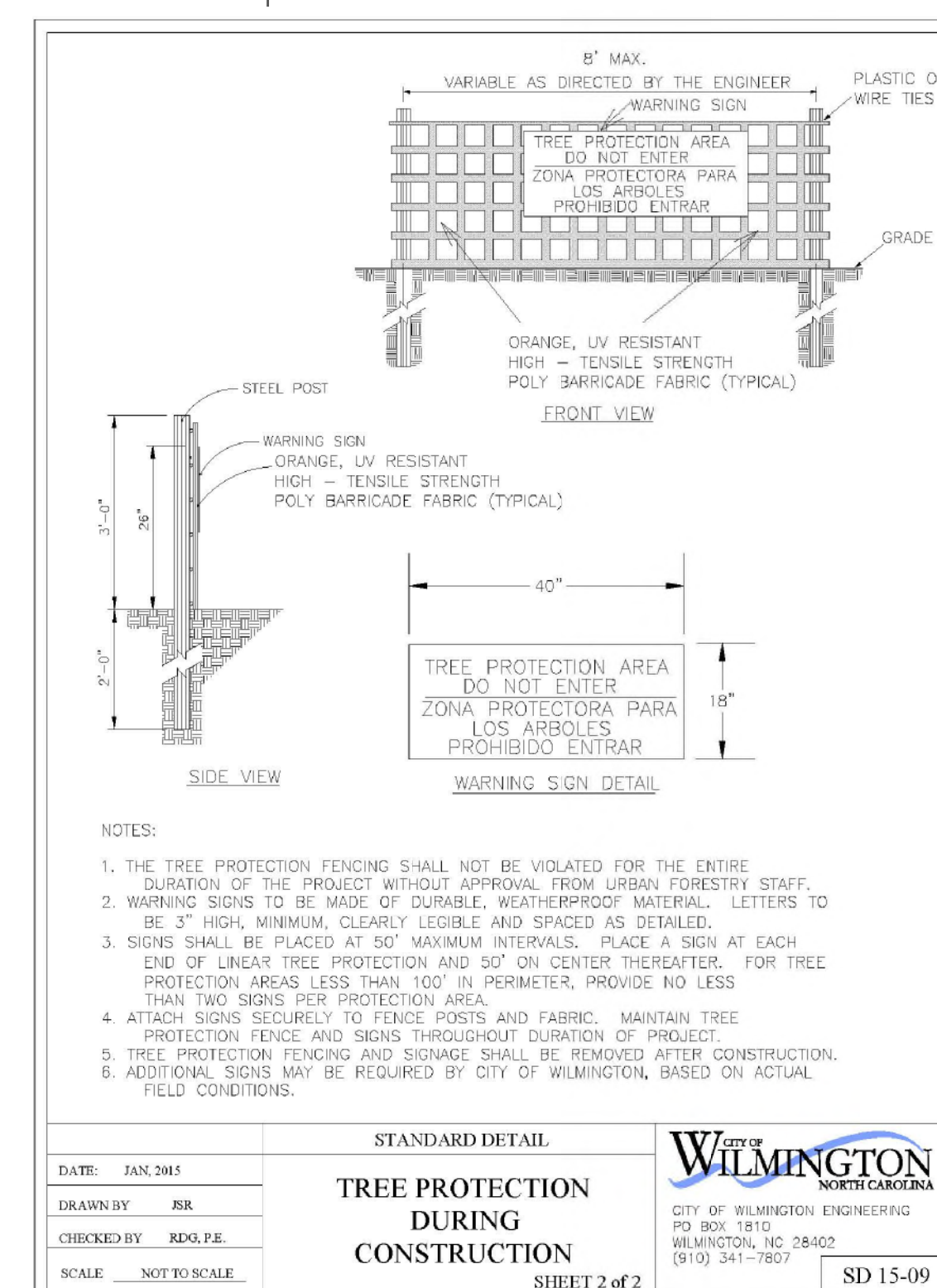
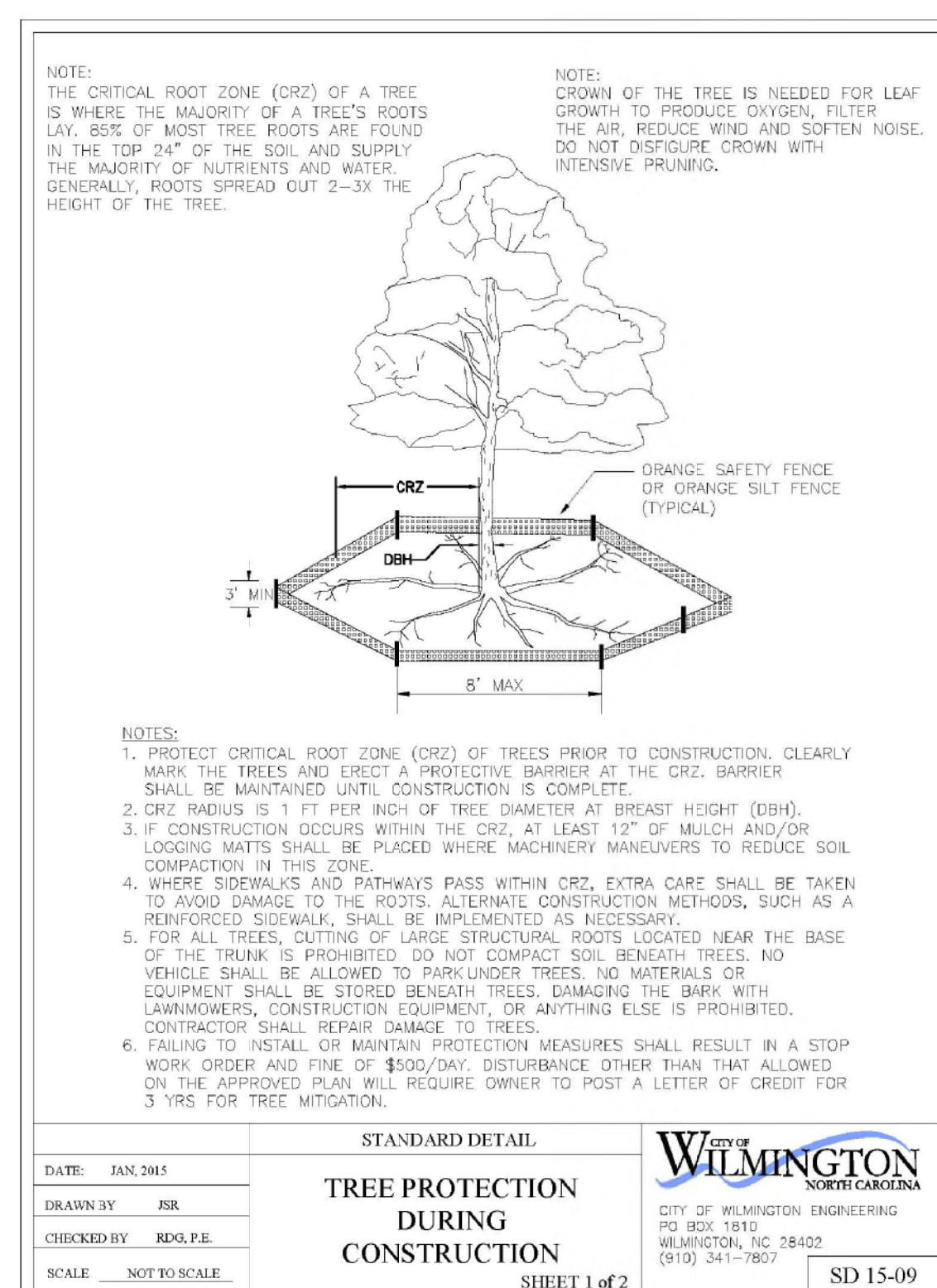
Quantity of Trees	Caliper inches/ Tree	Tree Common Name	Tree Type	% Mitigation	Total Cal. Inches
<b>Significant Trees</b>					
<b>Category I Mitigation (100%)</b>					
3	24	MAPLE	hardwood	100	72
1	28	MAPLE	hardwood	100	28
1	24	OAK	hardwood	100	24
<b>Category II Mitigation (75%)</b>					
3	8	CHERRY	ornamental flowering	75	24
3	9	CHERRY	ornamental flowering	75	27
5	12	CHERRY	ornamental flowering	75	60
1	9	MAGNOLIA	ornamental flowering	75	9
1	11	MAGNOLIA	ornamental flowering	75	11
<b>Category III Mitigation (50%)</b>					
1	24	PINE	conifer	50	24
<b>Category IV Mitigation (25%)</b>					
1	8	MIMOSA	ornamental flowering	25	8
<b>Total Significant Tree Caliper Inches to be Removed on Site</b>					<b>287</b>

**Regulated Trees**

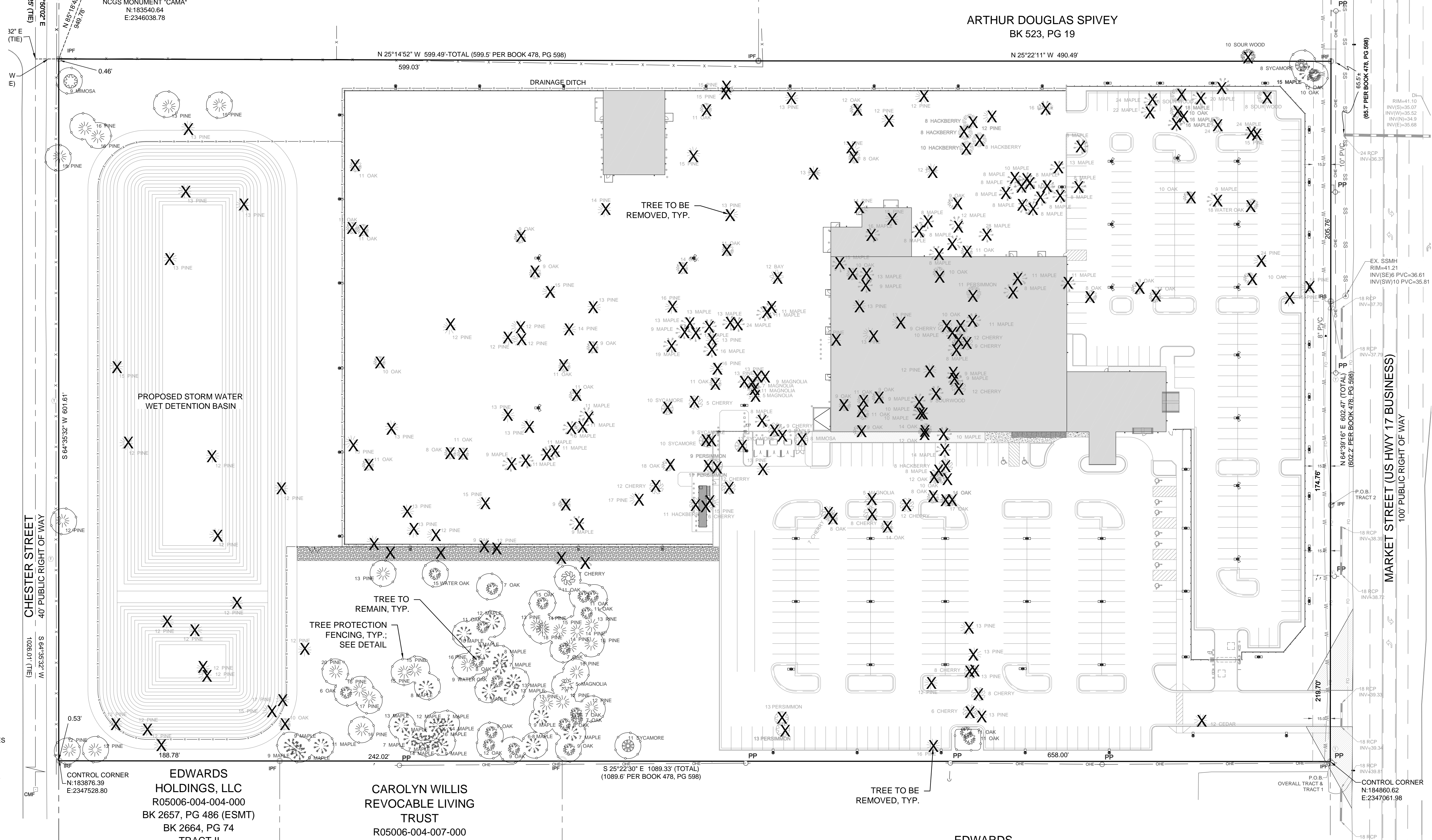
Quantity of Trees	Caliper inches/ Tree	Tree Common Name	Tree Type	% Mitigation	Total Cal. Inches
<b>Category I Mitigation (100%)</b>					
1	12	BAY	hardwood	100 assumed - not in table IV	12
4	8	HACKBERRY	hardwood	100 assumed - not in table IV	32
1	10	HACKBERRY	hardwood	100 assumed - not in table IV	10
1	11	HACKBERRY	hardwood	100 assumed - not in table IV	11
19	8	MAPLE	hardwood	100	152
10	9	MAPLE	hardwood	100	90
5	10	MAPLE	hardwood	100	50
10	11	MAPLE	hardwood	100	110
1	12	MAPLE	hardwood	100	12
6	13	MAPLE	hardwood	100	78
1	14	MAPLE	hardwood	100	14
1	15	MAPLE	hardwood	100	15
6	16	MAPLE	hardwood	100	96
1	18	MAPLE	hardwood	100	18
1	19	MAPLE	hardwood	100	19
1	20	MAPLE	hardwood	100	20
1	22	MAPLE	hardwood	100	22
5	8	OAK	hardwood	100	40
12	9	OAK	hardwood	100	108
9	10	OAK	hardwood	100	90
13	11	OAK	hardwood	100	143
3	12	OAK	hardwood	100	36
5	14	OAK	hardwood	100	70
1	17	OAK	hardwood	100	17
2	18	OAK	hardwood	100	36
1	8	SOURWOOD	hardwood	100	8
1	9	SOURWOOD	hardwood	100	9
1	10	SOURWOOD	hardwood	100	10
<b>Category II Mitigation (75%)</b>					
1	12	CEDAR	conifer	75	12
1	9	SYCAMORE	hardwood	75	9
1	13	SYCAMORE	hardwood	75	13
<b>Category III Mitigation (50%)</b>					
1	9	PERSIMMON	hardwood	50	9
2	11	PERSIMMON	hardwood	50	22
2	13	PERSIMMON	hardwood	50	26
25	12	PINE	conifer	50	300
29	13	PINE	conifer	50	377
4	14	PINE	conifer	50	56
11	15	PINE	conifer	50	165
3	16	PINE	conifer	50	48
2	17	PINE	conifer	50	34
<b>Total Regulated Tree Caliper Inches to be Removed on Site</b>					<b>2399</b>
<b>Total Significant Tree Caliper Inches to be Removed on Site</b>					<b>287</b>
<b>Total Caliper Inches to be Removed on Site</b>					<b>2686</b>

**TREE MITIGATION CALCULATIONS**  
 SIGNIFICANT TREE MITIGATION TOTAL:  
 CATEGORY 1 (100% MITIGATION): (63) 3" CAL. TREES  
 CATEGORY 2 (75% MITIGATION): (66) 3" CAL. TREES  
 CATEGORY 3 (50% MITIGATION): (8) 3" CAL. TREES  
 CATEGORY 4 (25% MITIGATION): (2) 3" CAL. TREES  
 TREE PRESERVATION CREDIT:  
 TOTAL CALIPER INCHES OF TREES TO BE PRESERVED: 944 CAL. IN. / 6 (FROM TABLE 1 IN SECTION 10-48 OF CITY OF WILMINGTON LDC) = 157 TREES  
 GRAND TOTAL REPLACEMENT TREES FOR MITIGATION: (2) 3" CAL. TREES = 159 TREES - 157 TREES  
 - ALL TREES TO BE REMOVED ARE INCLUDED AS PART OF NECESSARY SITE IMPROVEMENTS.

**TREE REMOVAL NOTES:**  
 1. PRIOR TO ANY CLEARING, GRADING, OR CONSTRUCTION ACTIVITY, TREE PROTECTION FENCING WILL BE INSTALLED AROUND PROTECTED TREES OR GROVES OF TREES. NO CONSTRUCTION WORKERS, TOOLS, MATERIALS OR VEHICLES ARE PERMITTED WITHIN TREE PROTECTION FENCING.



ARTHUR DOUGLAS SPIVEY TRUST  
 R05006-004-009-000  
 TRACT ONE  
 BK 5715, PG 944  
 BK 1174, PG 1937



**NOT RELEASED FOR CONSTRUCTION**

**LATEST DATE:** DA16-026/PC16-064  
**DRAWN BY:** RPB  
**CHECKED BY:** RPB

REV#	DATE	DESCRIPTION	BY
09	MAR 17	REVISION #1	

**CITY OF WILMINGTON**  
 NORTH CAROLINA  
 Public Services • Engineering Division  
 APPROVED STORMWATER MANAGEMENT PLAN

Date: \_\_\_\_\_ Permit #: \_\_\_\_\_  
 Signed: \_\_\_\_\_

**Approved Construction Plan**

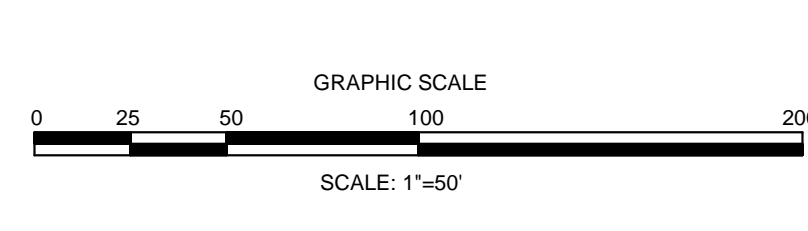
Name \_\_\_\_\_ Date \_\_\_\_\_

Planning \_\_\_\_\_  
 Traffic \_\_\_\_\_  
 Fire \_\_\_\_\_

For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance.

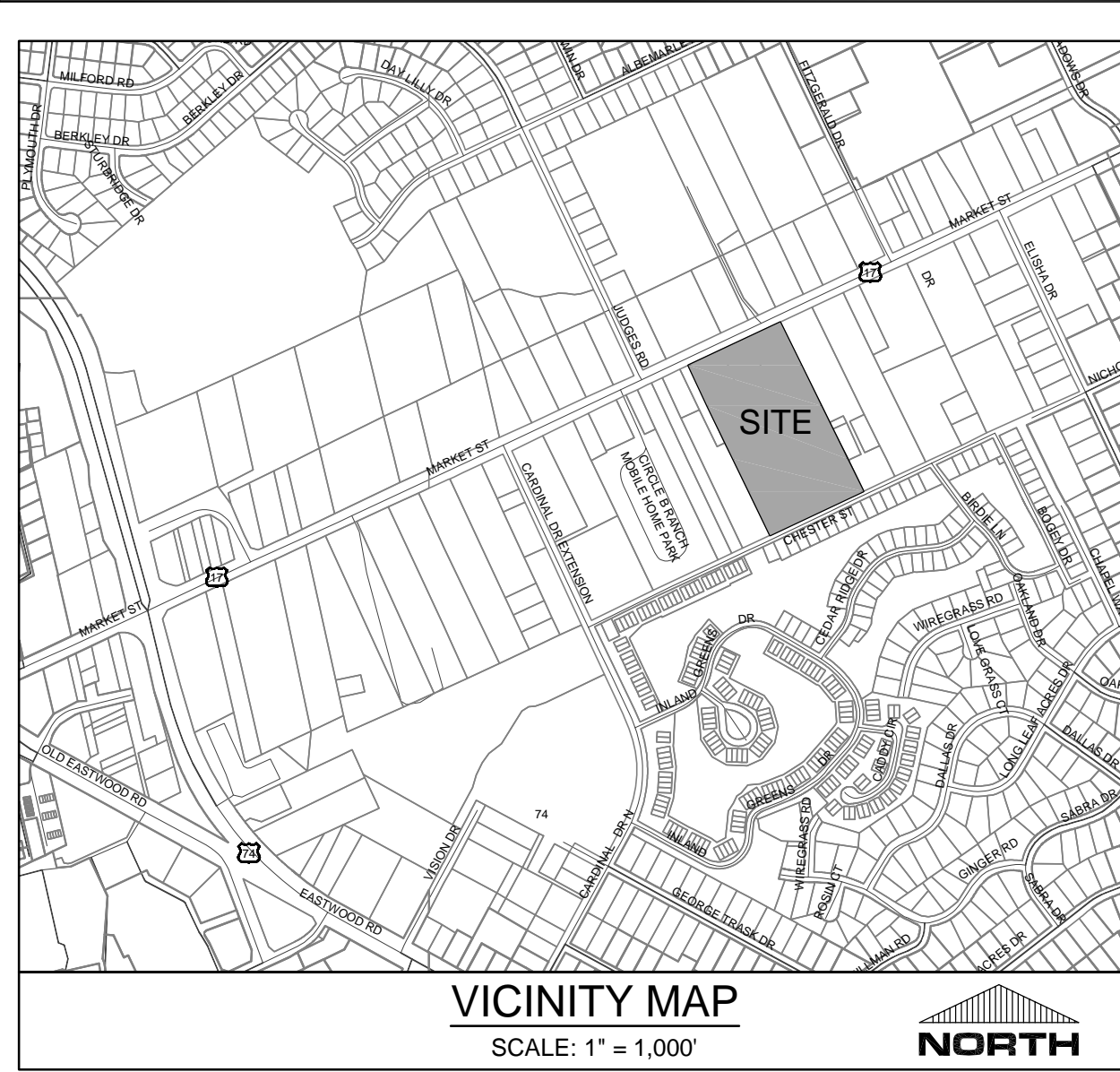
**811**  
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NCDENR PWSS WATER PERMIT #: \_\_\_\_\_ GPD  
 WATER CAPACITY: \_\_\_\_\_ GPD  
 DWQ SEWER PERMIT #: \_\_\_\_\_  
 SEWER CAPACITY: \_\_\_\_\_ GPD  
 SEWER SHED # AND PLANT: \_\_\_\_\_  
 SEWER TO FLOW THROUGH NET: YES or NO (CIRCLE ONE)



**CARMAX**  
 THE AUTO SUPERSTORE  
 STORE NUMBER 6022  
 6030 MARKET STREET  
 WILMINGTON, NC 28405

**PROJECT NO.:** 20-15081.02  
**DATE:** 11 NOV 16  
**SHEET TITLE:** TREE REMOVAL PLAN  
**SHEET NO.:** C-2.3



**ASPHALT AREA NOTE:**  
 1. SITE CONTRACTOR SHALL STRIP TOPSOIL AND ANY UNSUITABLE MATERIAL AND PROVIDE CUTFILL OPERATIONS TO PROVIDE A COMPACTED CONTROLLED SUBGRADE, IN ACCORDANCE WITH THE SUBSURFACE GEOTECHNICAL EXPLORATION AND TECHNICAL SPECIFICATIONS.

**BUILDING PAD NOTE:**  
 1. SITE CONTRACTOR SHALL STRIP TOPSOIL AND ANY UNSUITABLE MATERIAL AND PROVIDE CUTFILL OPERATIONS TO PROVIDE A COMPACTED CONTROLLED BUILDING PAD, IN ACCORDANCE WITH THE SUBSURFACE GEOTECHNICAL EXPLORATION AND TECHNICAL SPECIFICATIONS.

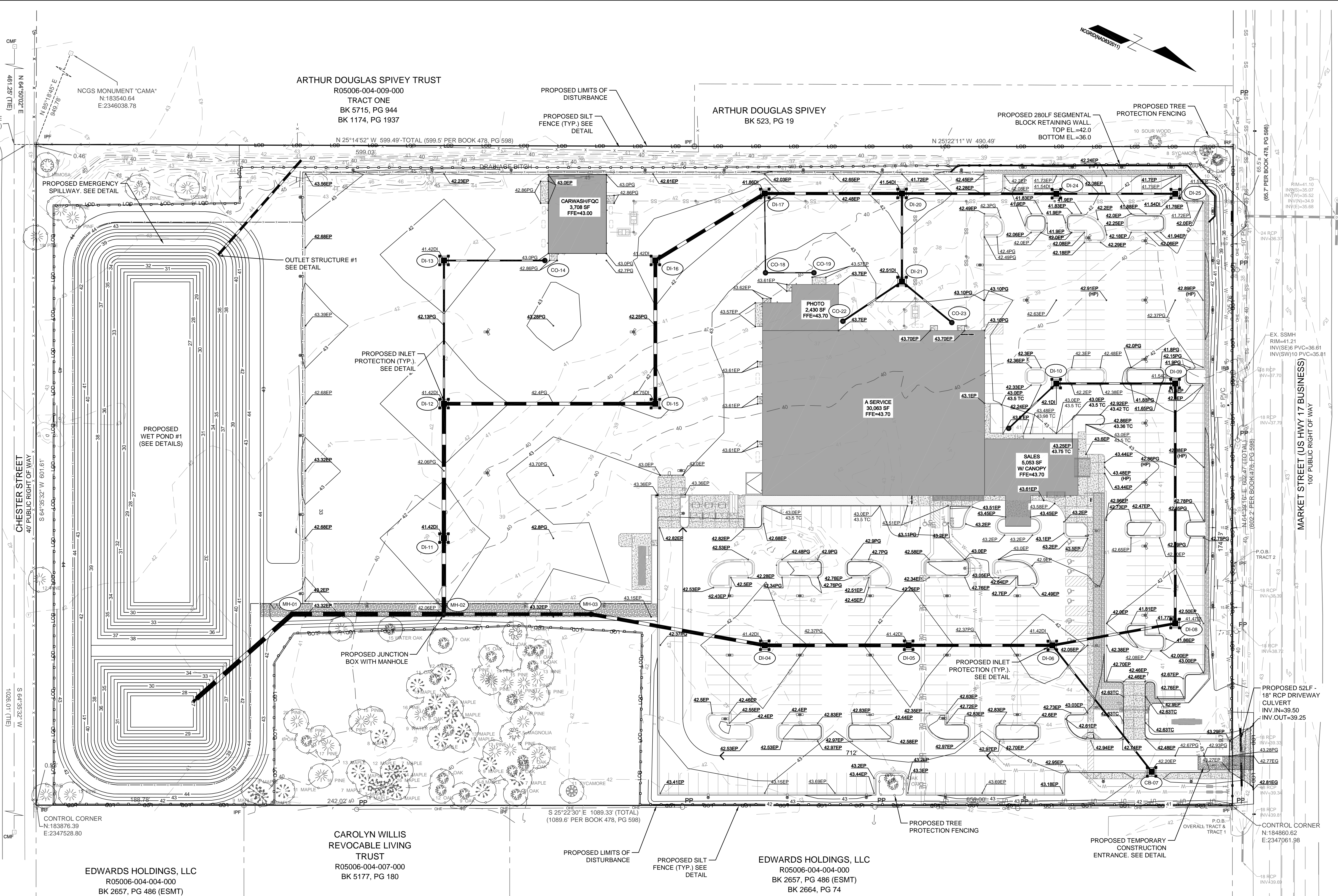
**STORMWATER NOTE:**  
 1. THIS PROJECT DRAINS TO A PROPOSED ON-SITE STORM WET DETENTION BASIN.  
 2. ONCE SITE IS STABILIZED AND SEDIMENT BASIN IS CONVERTED TO WET DETENTION BASIN, CONTRACTOR SHALL REMOVE ALL SEDIMENT FROM BASIN AND RESTORE TO DESIGN ELEVATIONS.

**GENERAL NOTES:**  
 1. ALL CONSTRUCTION TO BE IN ACCORDANCE WITH ALL STATE OF NC, CITY OF WILMINGTON, AND NEW HANOVER COUNTY STANDARDS AND SPECIFICATIONS.

2. THE CONTRACTOR SHALL PLACE INLET PROTECTION AROUND ALL STORM DRAIN INLETS TO PROTECT THE SYSTEM FROM COLLECTING SEDIMENTATION DURING CONSTRUCTION. INLET PROTECTION SHALL REMAIN IN PLACE UNTIL THE PARKING LOT IS PAVED.  
 3. CONTRACTOR SHALL ADJUST ALL FRAMES OF EX. UTILITY INFRASTRUCTURE WITHIN ASPHALT OVERLAY AND NEW ASPHALT AREAS TO MATCH PROPOSED GRADES.  
 4. ALL PROPOSED SPOT ELEVATIONS SHOWN IN THE PARKING LOT ARE PROPOSED EDGE OF PAVEMENT OR FACE OF CURB @ GUTTER FOR CURB SECTIONS, UNLESS NOTED OTHERWISE.  
 5. ALL ROOF DRAINS SHALL BE DIRECTED TOWARDS ON-SITE COLLECTION SYSTEM. NO ROOF DRAINS SHALL BE DIRECTED OFF-SITE WITHOUT ENTERING THE ON-SITE COLLECTION FIRST.  
 6. ALL SIDEWALK CROSS SLOPES HAVE BEEN GRADED TO MEET ADA REGULATIONS. CONTRACTOR SHALL CONFIRM GRADES BEFORE PLACING PAVEMENT OR SIDEWALKS AND REPORT ANY DISCREPANCIES TO OWNER AND/OR ENGINEER.  
 7. THE CONTRACTOR SHALL USE EITHER RCP (CL III) OR HDPE PIPE FOR THE STORM DRAINAGE SYSTEM. IF THE CONTRACTOR CHOOSES TO USE HDPE PIPE, IT SHALL BE ADS N-12 WT STORM PIPE OR APPROVED EQUAL AND SHALL BE INSTALLED TO MANUFACTURER SPECIFICATIONS. IN ADDITION, THE CONTRACTOR SHALL BE SURE TO PROVIDE CONCRETE COLLARS AROUND EACH F.E.S. TO PREVENT FLOATATION IF HDPE PIPE IS CHOSEN.

8. CONTRACTOR SHALL REFER TO THE GEOTECHNICAL ENGINEERING REPORT FOR CARMAX - WILMINGTON, NC (CAPE FEAR), PREPARED BY ECS CAROLINAS, LLP, PROJECT NUMBER: 22-22866-REVISED, AND DATED AUGUST 20, 2015 FOR SITE CONSTRUCTION RECOMMENDATIONS.

9. CONTRACTOR SHALL STAKE SILT FENCE ALONG LIMITS OF DISTURBANCE LINE. THE SILT FENCE LINETYPE IS OFFSET ON THE DRAWING FOR CLARITY.



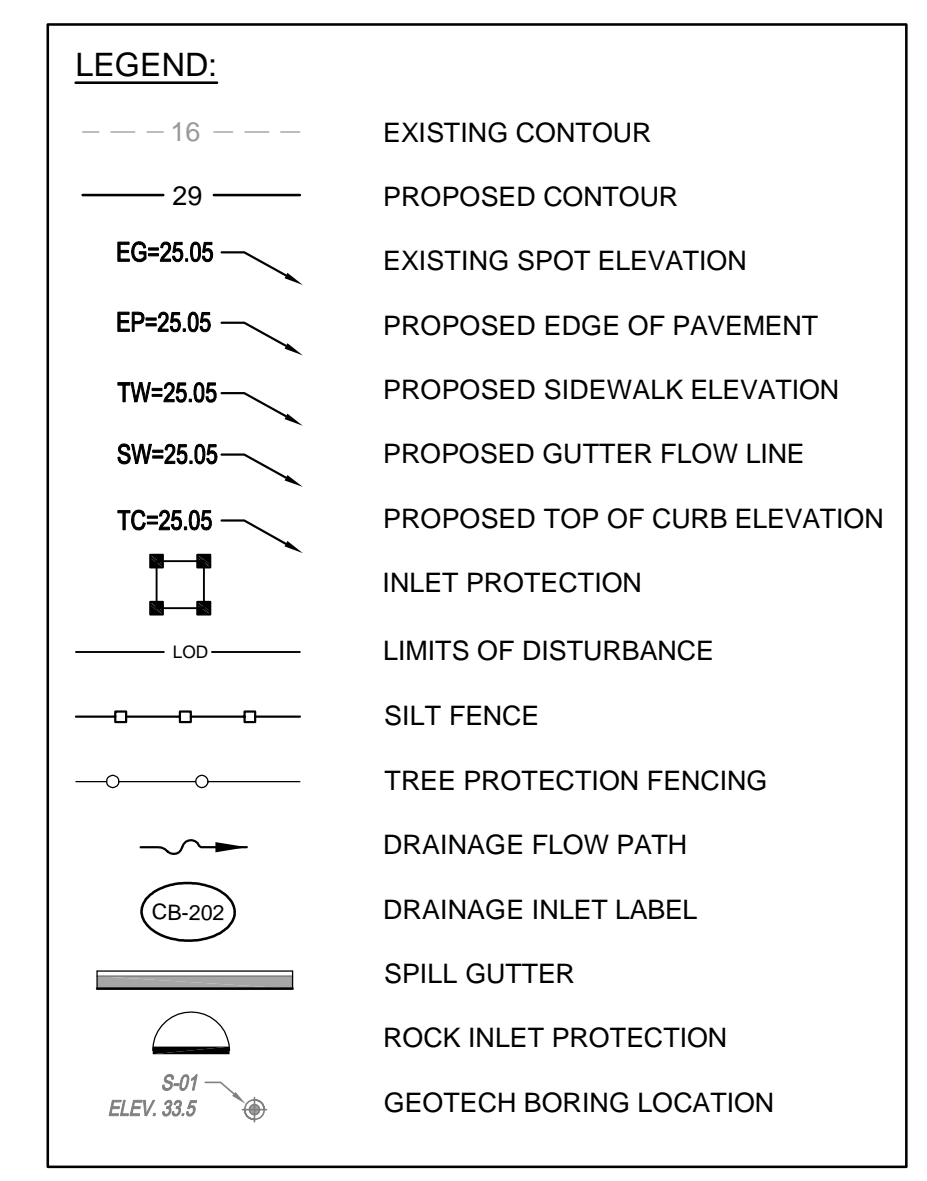
**EDWARDS HOLDINGS, LLC**  
 R05006-004-004-000  
 BK 2657, PG 486 (ESMT)  
 BK 2664, PG 74  
 TRACT II

**CAROLYN WILLIS REVOCABLE LIVING TRUST**  
 R05006-004-007-000  
 BK 5177, PG 180

**EDWARDS HOLDINGS, LLC**  
 R05006-004-004-000  
 BK 2657, PG 486 (ESMT)  
 BK 2664, PG 74  
 TRACT II

**STORM SEWER SCHEDULE:**

Upstream Node	Downstream Node	Diameter (in)	Upstream Pipe Invert (ft)	Downstream Pipe Invert (ft)	Pipe Length (ft)	Slope (%)	Upstream Rim Elev (ft)	Downstream Rim Elev (ft)	Pipe Material
CO-14	DI-13	15	33.25	32.79	92	0.50	42.80	41.42	CL III RCP
DI-13	DI-12	18	32.79	32.14	130	0.50	41.42	41.42	CL III RCP
DI-12	DI-11	42	32.14	31.52	122	0.50	41.42	41.42	CL III RCP
DI-11	MH-02	42	31.52	31.15	69	0.54	41.42	42.06	CL III RCP
MH-02	MH-01	48	31.15	30.46	138	0.50	42.06	43.25	CL III RCP
MH-01	FES-100	48	30.46	27.50	116	2.54	43.25	38.00	CL III RCP
CO-19	CO-18	8	37.42	37.20	44	0.50	43.51	43.21	CL III RCP
CO-18	DI-17	8	37.20	36.84	72	0.50	43.21	41.86	CL III RCP
DI-17	DI-16	36	36.84	34.25	117	0.50	41.86	41.42	CL III RCP
DI-16	DI-15	36	34.25	33.60	130	0.50	41.42	41.75	CL III RCP
DI-15	DI-12	36	33.60	32.64	192	0.50	41.75	41.42	CL III RCP
CO-22	DI-21	15	36.19	35.86	65	0.50	43.62	42.51	CL III RCP
DI-21	DI-20	18	35.86	35.46	80	0.50	42.51	41.54	CL III RCP
DI-20	DI-17	30	35.46	34.84	125	0.50	41.54	41.86	CL III RCP
CO-23	DI-21	15	36.15	35.86	59	0.50	43.62	42.51	CL III RCP
CO-26	DI-10	12	40.00	35.94	60	6.82	43.49	42.10	CL III RCP
DI-10	DI-09	18	35.94	35.45	108	0.45	42.10	41.54	CL III RCP
DI-09	DI-08	24	35.45	34.36	218	0.50	41.54	41.47	CL III RCP
DI-08	DI-06	24	34.36	33.77	114	0.52	41.47	41.42	CL III RCP
DI-06	DI-05	30	33.77	33.13	131	0.49	41.42	41.42	CL III RCP
DI-05	DI-04	36	33.13	32.46	131	0.51	41.42	41.42	CL III RCP
DI-04	MH-03	36	32.46	31.82	161	0.40	41.42	43.25	CL III RCP
MH-03	MH-02	36	31.82	31.15	134	0.50	43.25	42.06	CL III RCP
DI-07	DI-06	15	34.61	33.77	146	0.57	42.20	41.42	CL III RCP
DI-25	DI-24	18	36.70	36.16	108	0.50	41.54	41.54	CL III RCP
DI-24	DI-20	24	36.16	35.46	141	0.50	41.54	41.54	CL III RCP



**NOTATION:**

- CI = CURB INLET
- DI = DROP INLET
- CO = CLEANOUT
- DDI = DOUBLE DROP INLET
- MH = STORM DRAIN MANHOLE
- RD = ROOD DRAIN CLEANOUT
- FFE = FINISHED FLOOR ELEVATION
- BPE = BUILDING PAD ELEVATION

**CITY OF WILMINGTON**  
 NORTH CAROLINA  
 Public Services • Engineering Division  
 APPROVED STORMWATER MANAGEMENT PLAN

Date: \_\_\_\_\_ Permit # \_\_\_\_\_

Signed: \_\_\_\_\_

**Approved Construction Plan**

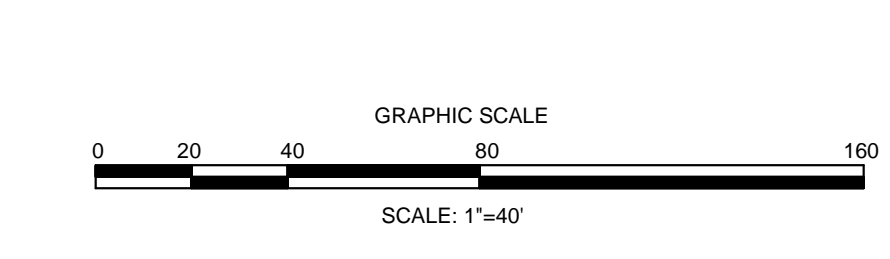
Name \_\_\_\_\_ Date \_\_\_\_\_

Planning \_\_\_\_\_  
 Traffic \_\_\_\_\_  
 Fire \_\_\_\_\_

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 SEWER CAPACITY: \_\_\_\_\_ GPD  
 SEWER SHED # AND PLANT: \_\_\_\_\_  
 SEWER TO FLOW THROUGH NEI: YES or NO (CIRCLE ONE)



**NOT RELEASED FOR CONSTRUCTION**

**LATEST DATE:** DA16-026/PC16-064

**DRAWN BY:** RPB

**CHECKED BY:** RPB

REVISIONS	REV#	DATE	DESCRIPTION	BY
	09	MAR 17	REVISION #1	

**CARMAX**  
 THE AUTO SUPERSTORE  
 STORE NUMBER 6022  
 6030 MARKET STREET  
 WILMINGTON, NC 28405

**PROJECT NO.:** 20-15081.02  
**DATE:** 25 APR 17  
**SHEET TITLE:** GRADING-DRAINAGE-PH. II EC PLAN  
**SHEET NO.:** C-3.1

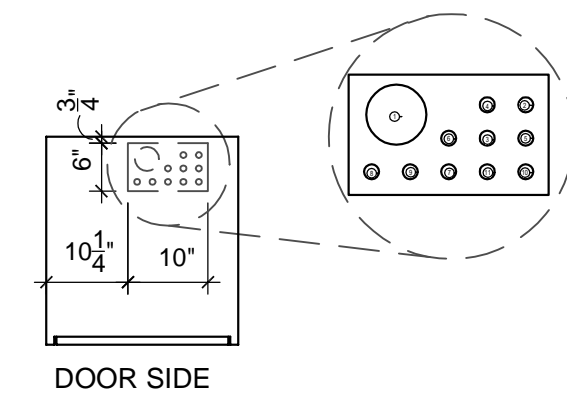
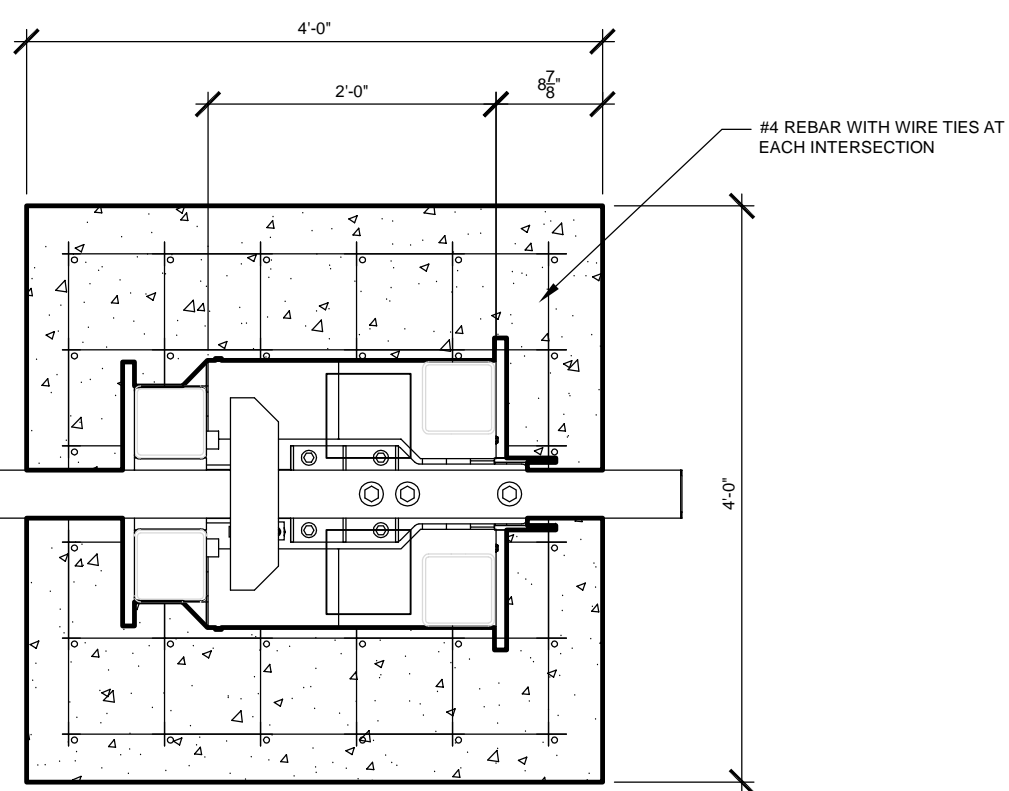
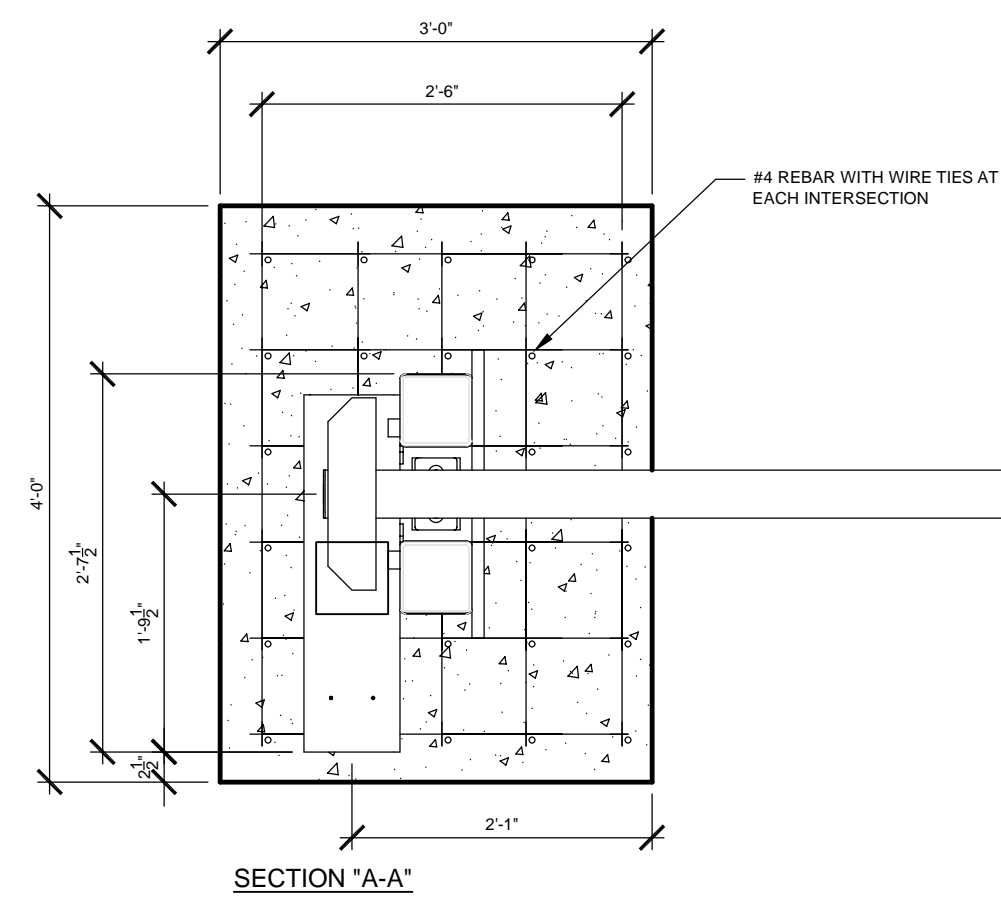
**PARAMOUNT ENGINEERING**  
 122 Cinema Drive  
 Wilmington, North Carolina 28403  
 (910) 791-6707 (F) (910) 791-6700 (F)  
 NC License # C-2846





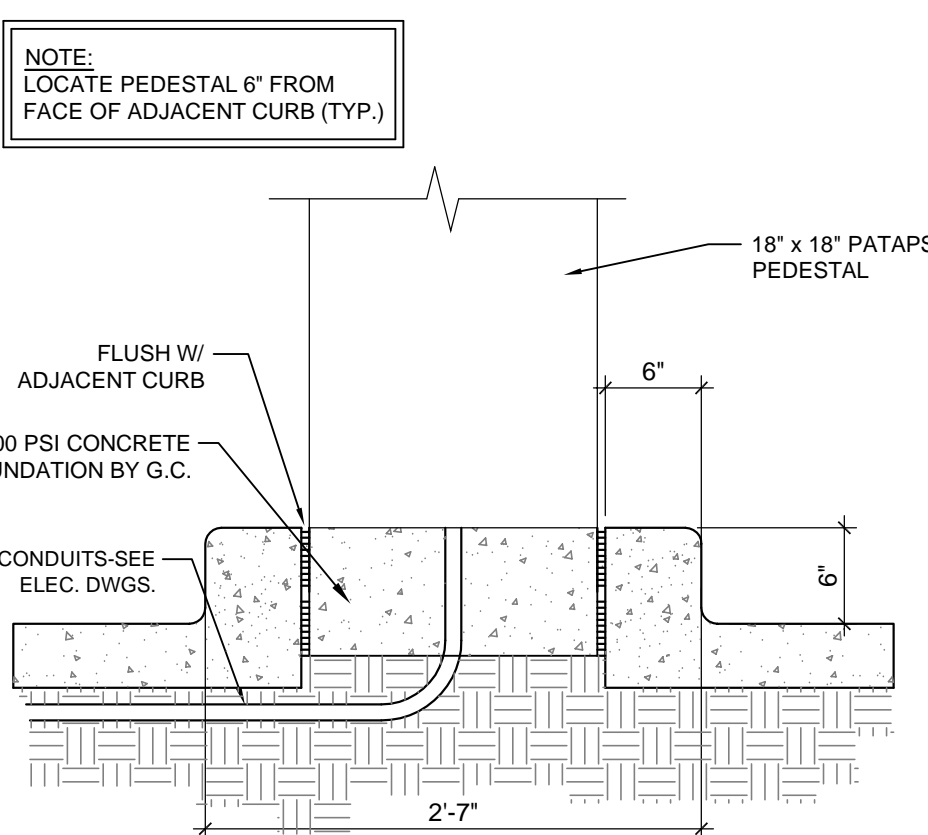




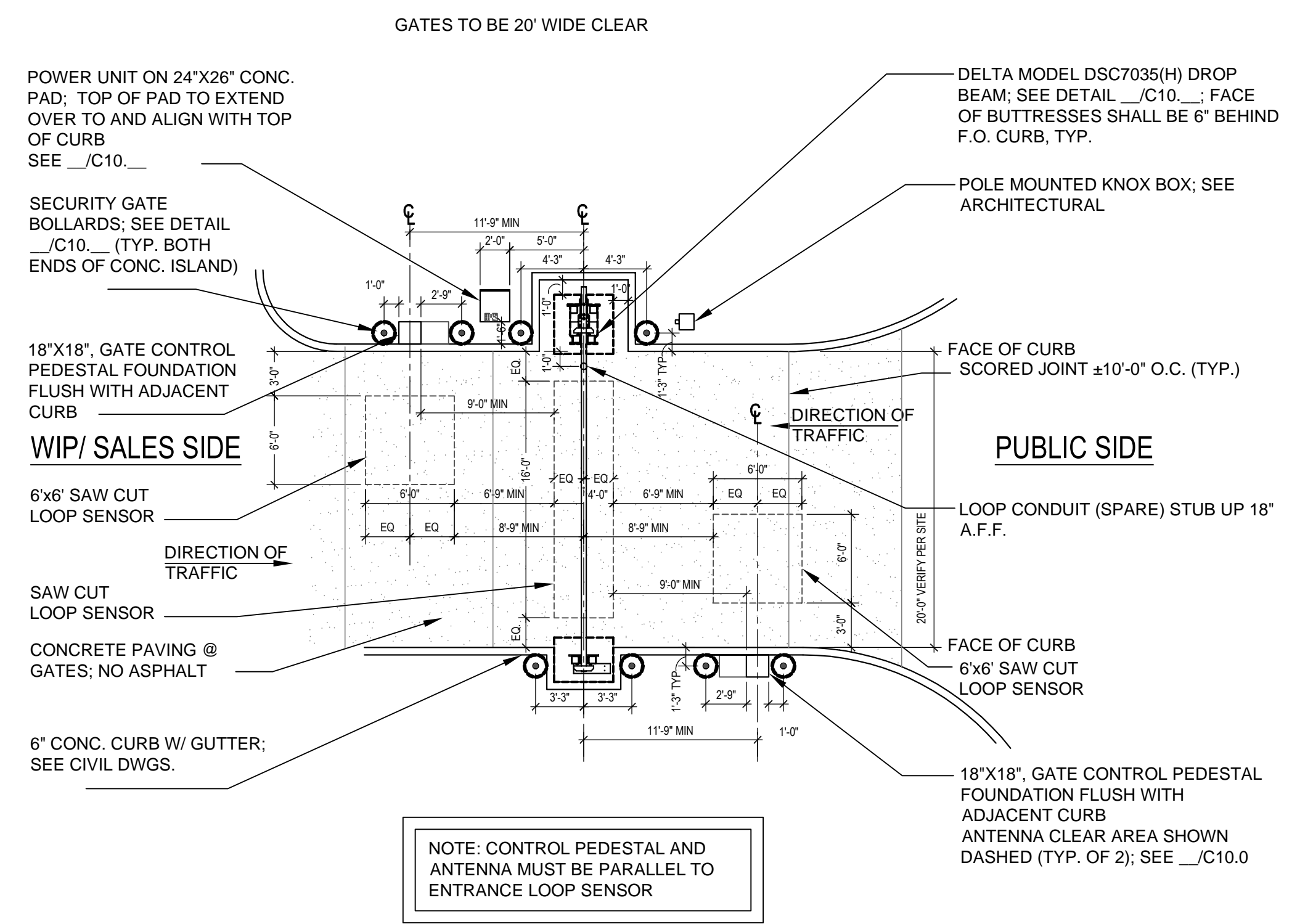


- DOOR SIDE**
- MUST MAINTAIN MIN. 2" SPACING BETWEEN ALL CONDUITS
  - CONDUITS MUST FIT IN THE LISTED SPACING (6" X 10")
  - 3" CONDUIT HYDRAULIC LINES
  - 1/2" CONDUIT FOR HEATER
  - 1/2" CONDUIT FOR LIMITS
  - 1/2" CONDUIT FOR ELECTRICAL LOCKING PIN
  - 1/2" CONDUIT FOR HEATER
  - 1" CONDUIT FOR SAW CUT LOOPS
  - 1" CONDUIT FOR SPARE LOOPS
  - 1" CONDUIT GATE PEDESTAL A
  - 1" CONDUIT GATE PEDESTAL B
  - 3 PHASE 208V CONDUIT SIZE DETERMINED BY ELECTRICIAN
  - SINGLE PHASE 120V CONDUIT SIZE DETERMINED BY ELECTRICIAN

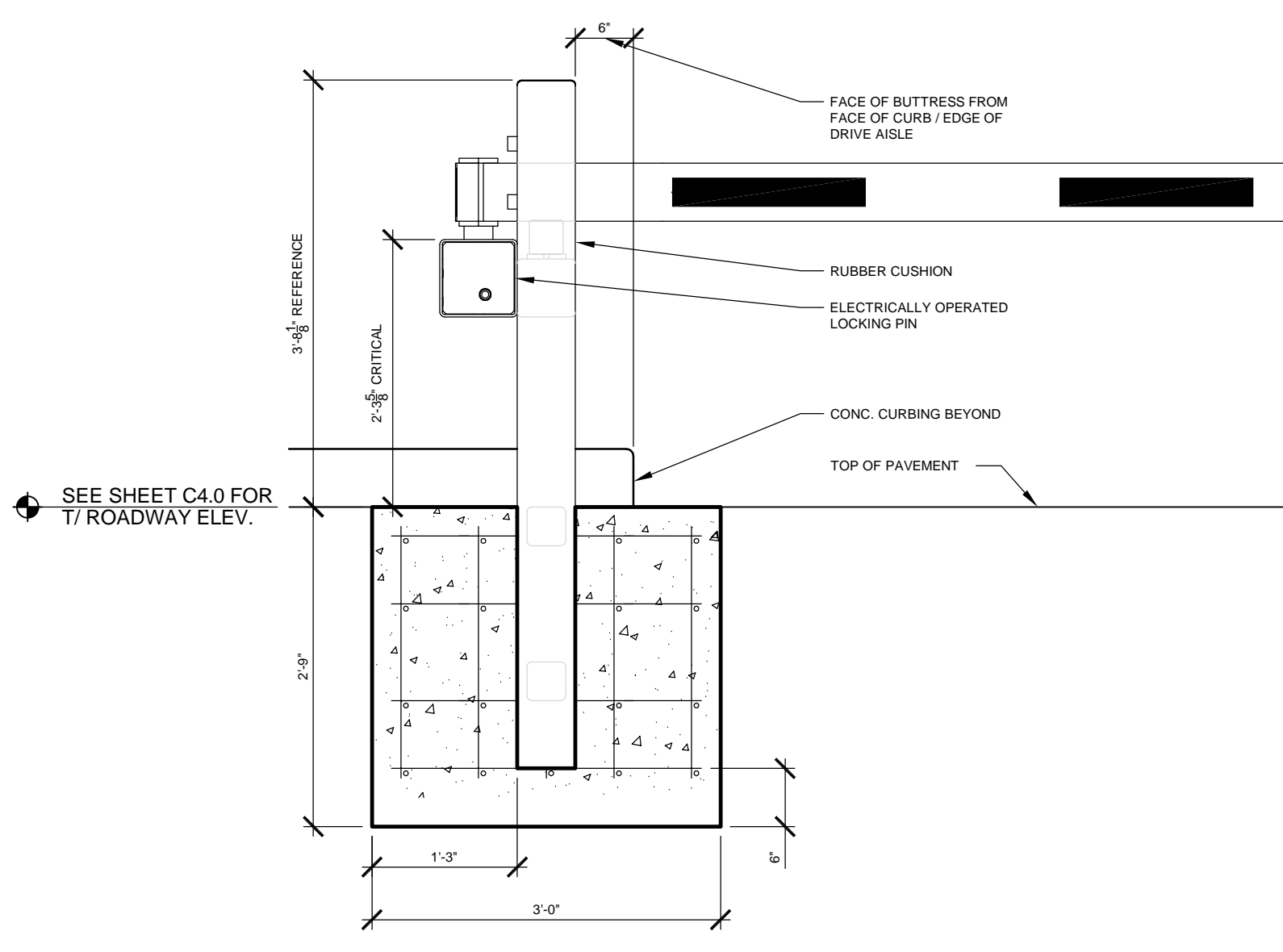
**SECURITY GATE POWER UNIT**  
NOT TO SCALE



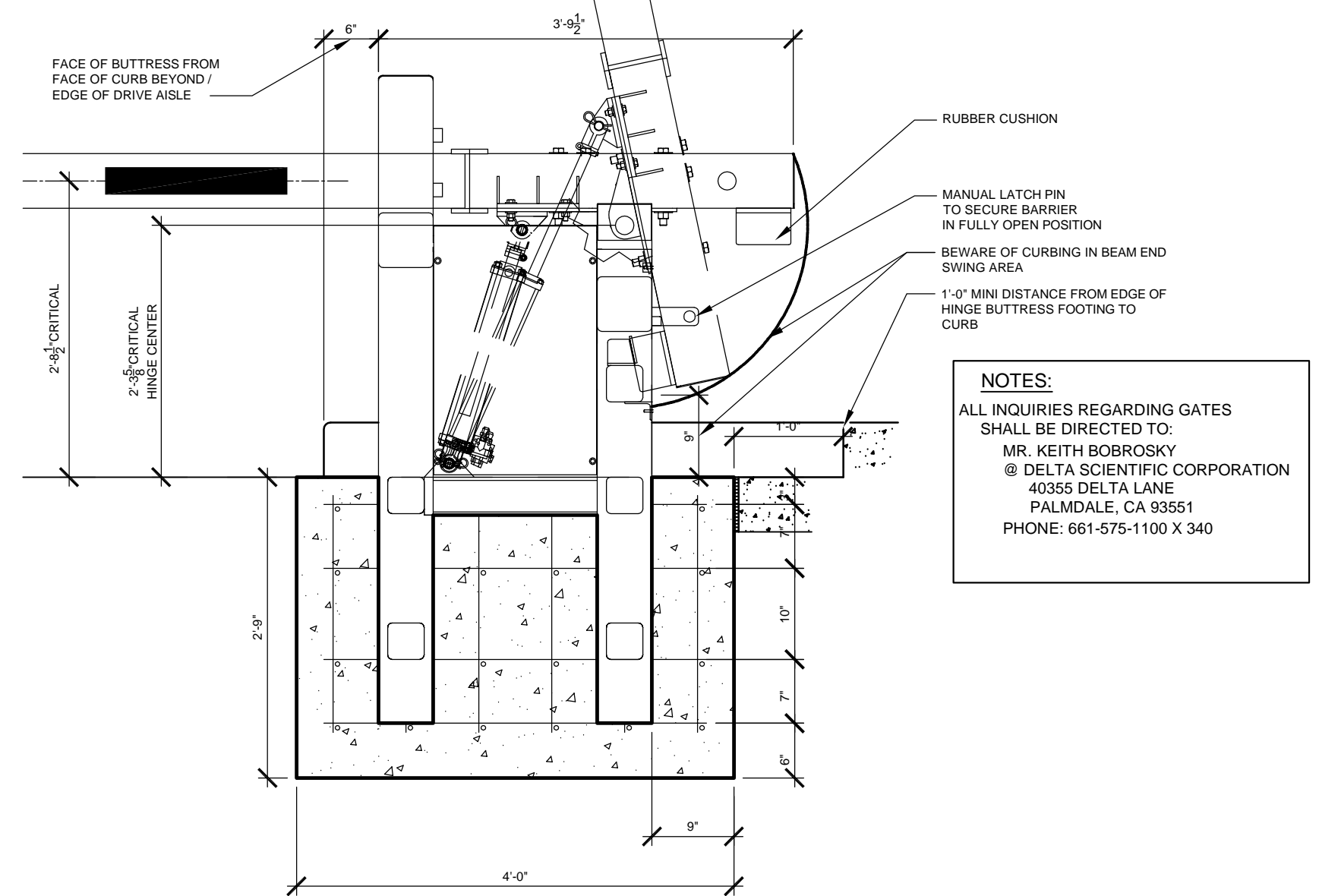
**PATAPSCO PEDESTAL**  
NOT TO SCALE



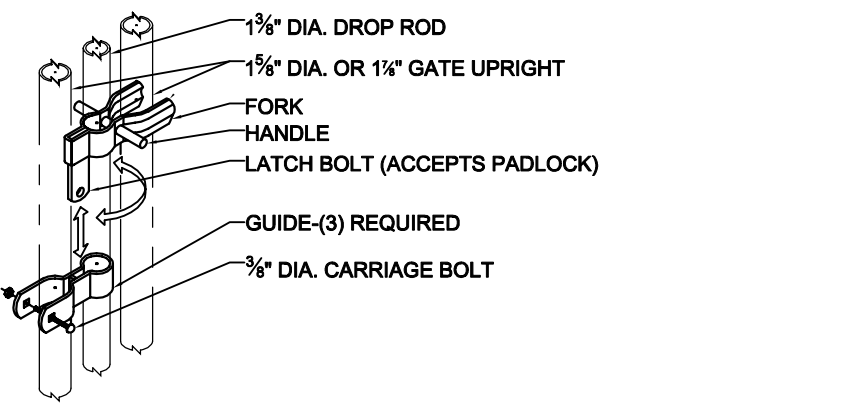
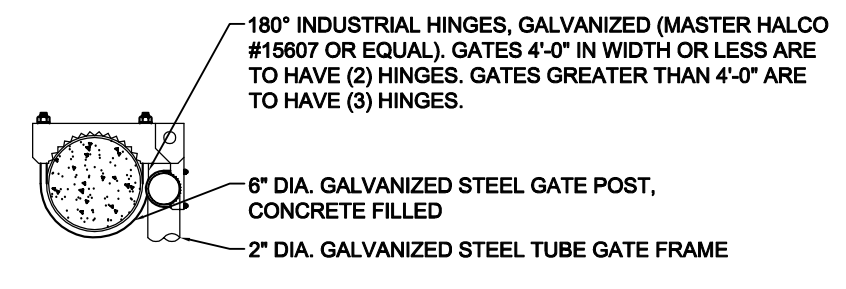
**SECURITY GATE (DROP BEAM)**  
PLAN VIEW  
NOT TO SCALE



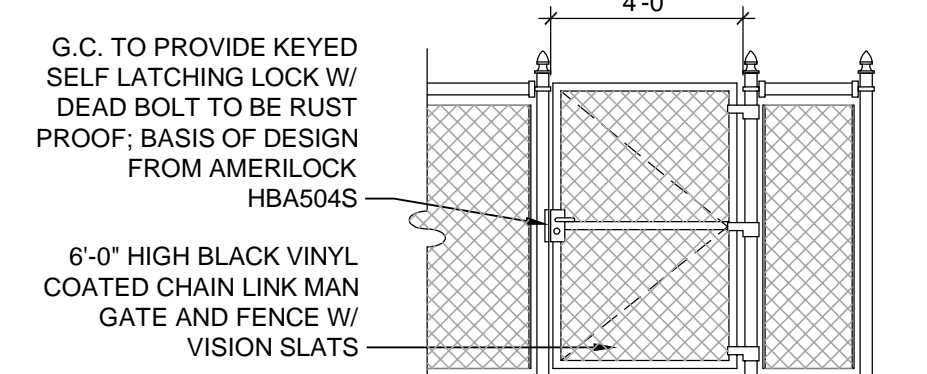
**SECURITY GATE DETAIL (DROP BEAM) FOR SINGLE GATE**  
NOT TO SCALE



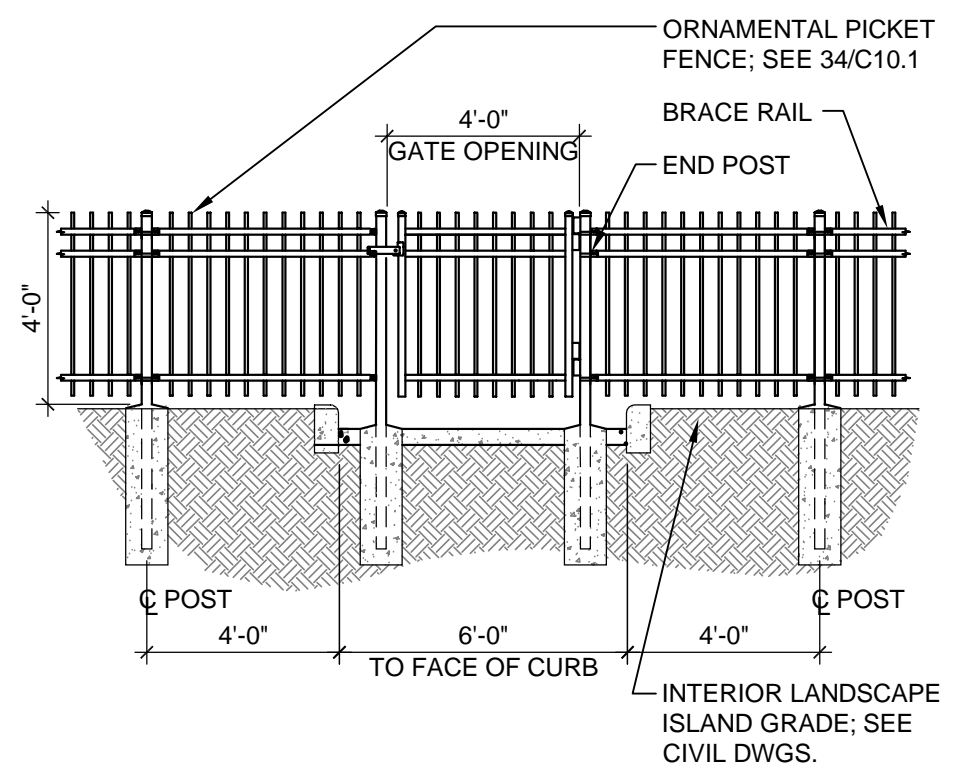
- NOTES:**  
ALL INQUIRIES REGARDING GATES SHALL BE DIRECTED TO:  
MR. KEITH BOBROVSKY  
@ DELTA SCIENTIFIC CORPORATION  
4035 DELTA LANE  
PALMDALE, CA 93551  
PHONE: 861-575-1100 X 340



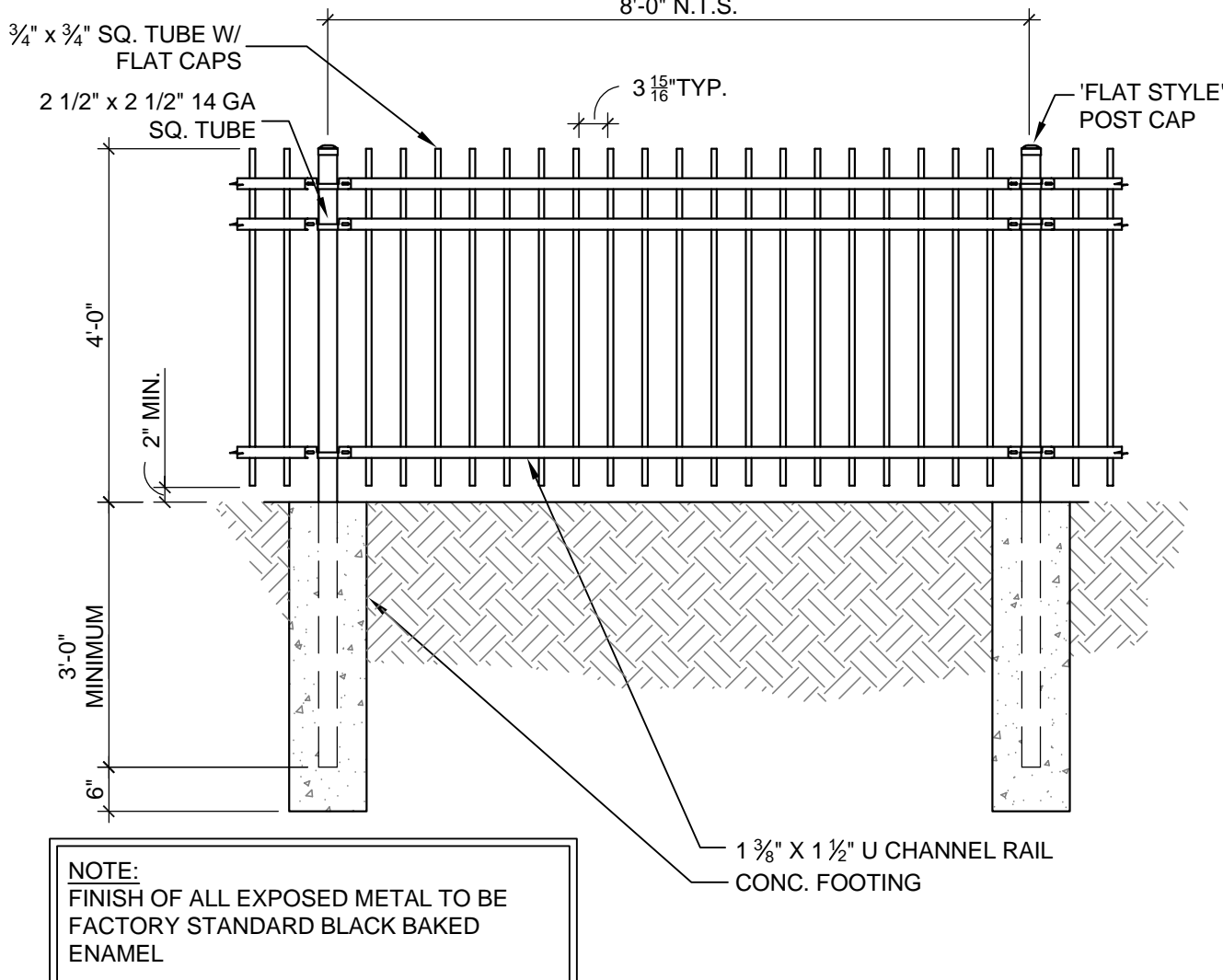
**HINGE AND LATCH DETAIL**  
NOT TO SCALE



**CHAINLINK GATE DETAIL**  
NOT TO SCALE

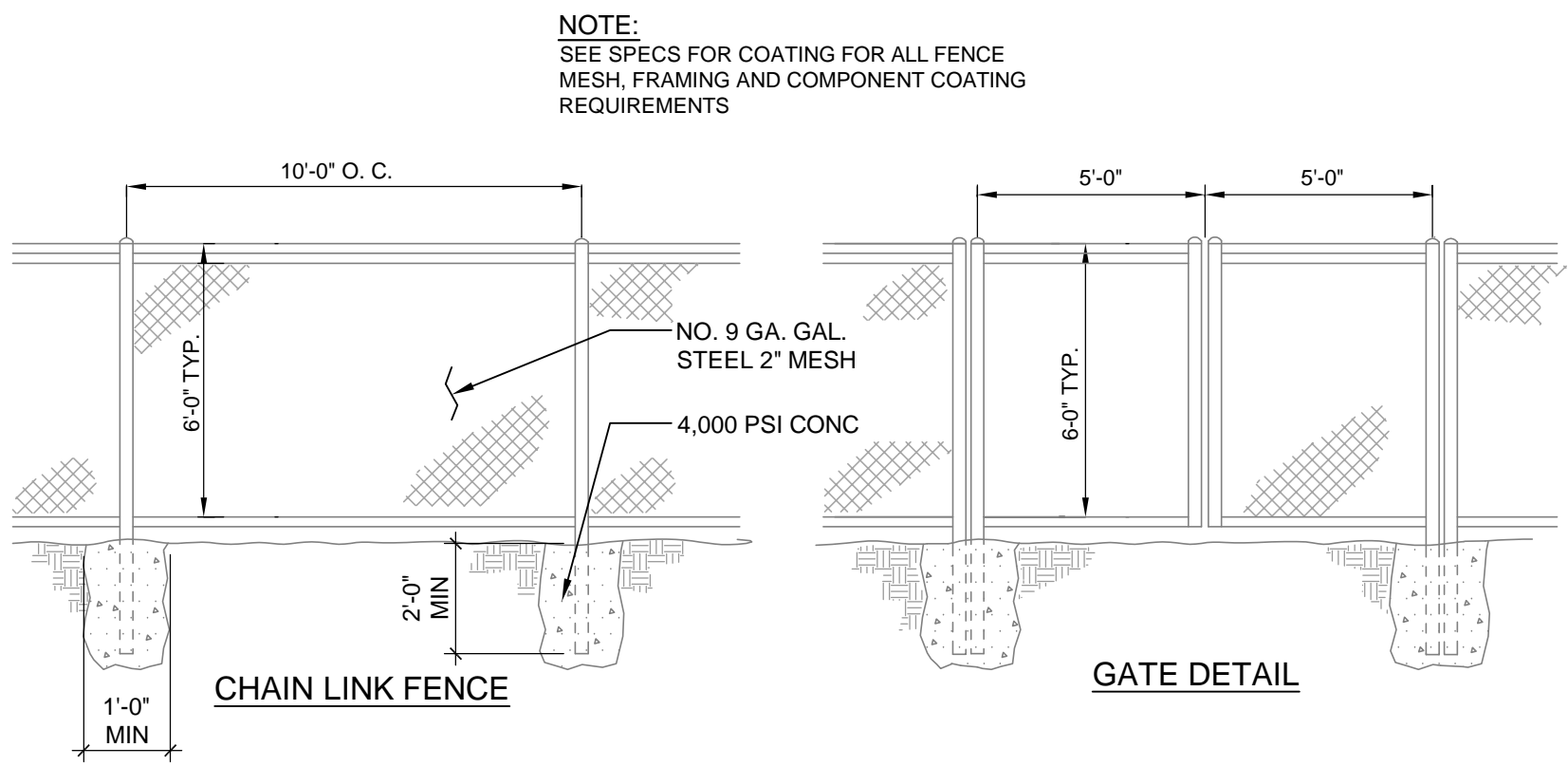


**ORNAMENTAL PICKET FENCE DETAIL**  
NOT TO SCALE

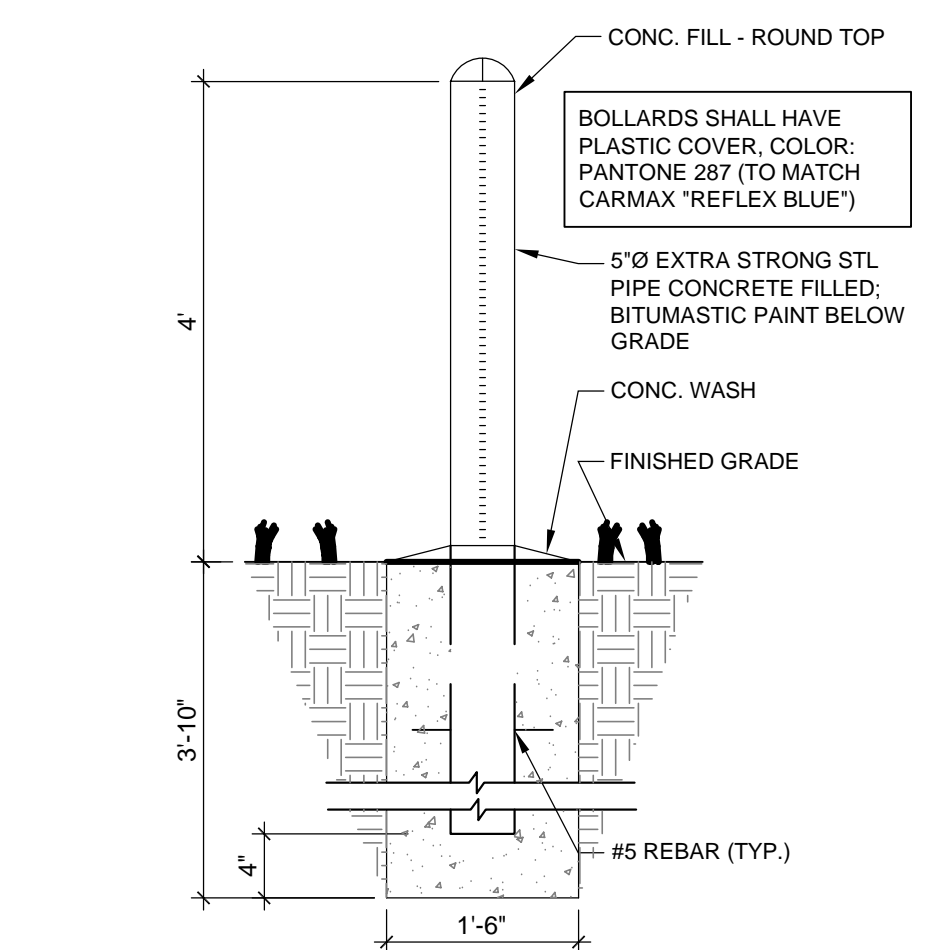


- NOTE:**  
FINISH OF ALL EXPOSED METAL TO BE FACTORY STANDARD BLACK BAKED ENAMEL

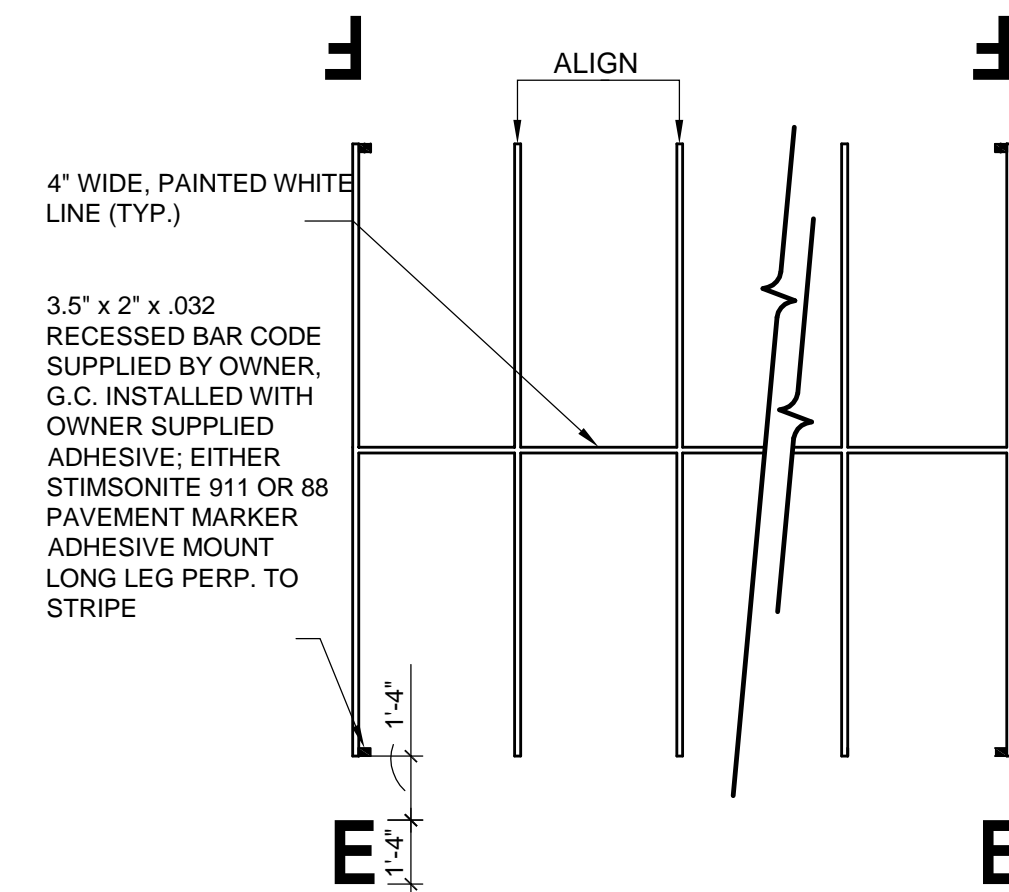
**ORNAMENTAL PICKET FENCE DETAIL**  
NOT TO SCALE



**CHAINLINK FENCE DETAIL**  
NOT TO SCALE

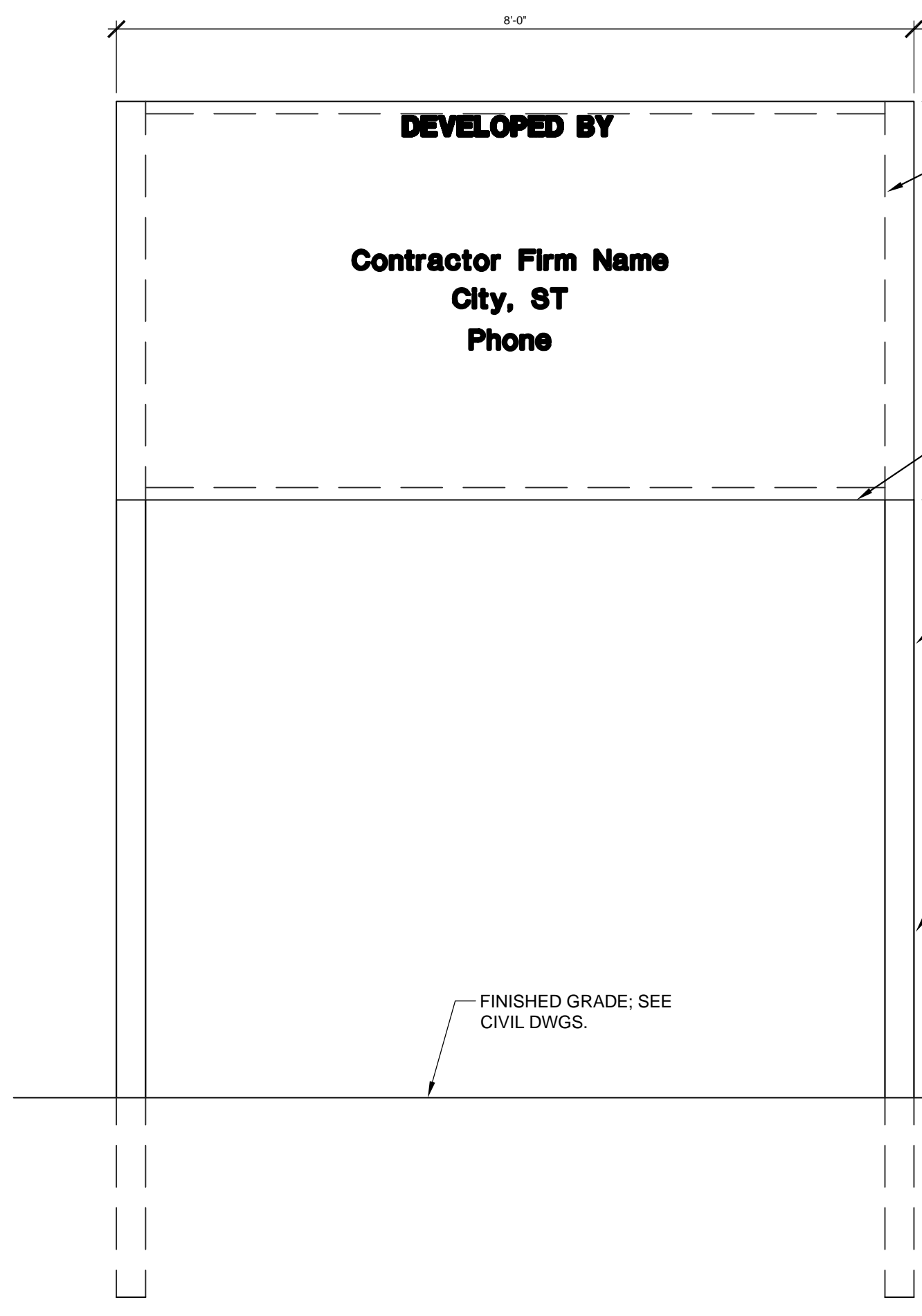


**SITE BOLLARD DETAIL**  
NOT TO SCALE

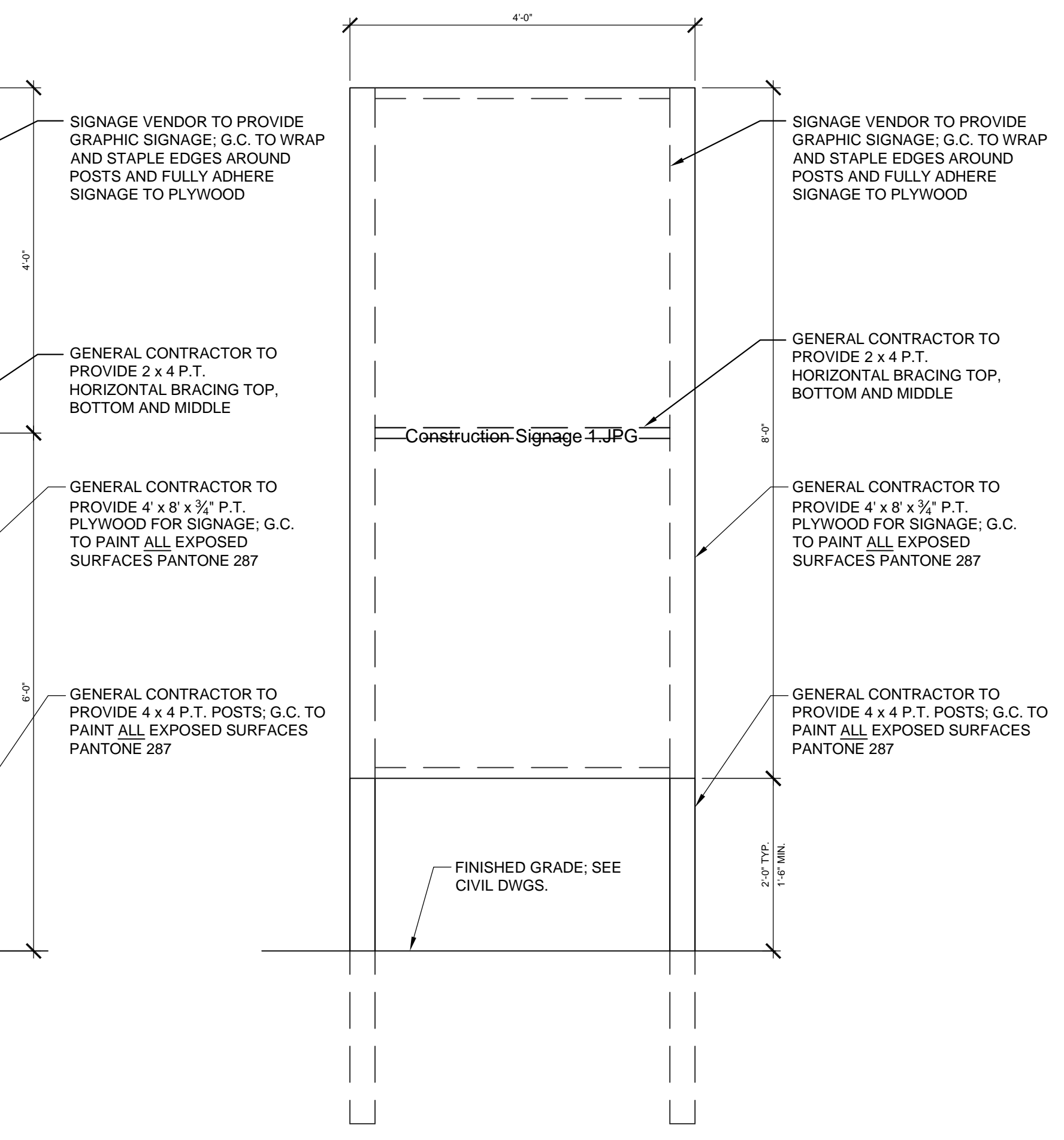


**TYP. SALES LOT IDENTIFIERS**  
NOT TO SCALE

- NOTE:**  
LETTERS SHALL BE LOCATED AT SALES PARKING STALLS AT EACH END OF A ROW, AT EACH SIDE OF THE PROMENADE, AND AT EACH SIDE OF AN ISLAND.  
BAR CODES SHALL BE LOCATED AT SALES PARKING STALLS AT EACH END OF A ROW AND AT EACH SIDE OF THE PROMENADE  
SEE ARCHITECTURAL SITE PLAN.



**COMING SOON SIGNS**  
NOT TO SCALE



**APPROVED CONSTRUCTION PLAN**

City of WILMINGTON, NORTH CAROLINA  
Public Services • Engineering Division  
APPROVED STORMWATER MANAGEMENT PLAN

Date: \_\_\_\_\_ Permit # \_\_\_\_\_  
Signed: \_\_\_\_\_

Name \_\_\_\_\_ Date \_\_\_\_\_

Planning \_\_\_\_\_  
Traffic \_\_\_\_\_  
Fire \_\_\_\_\_

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09	MAR 17	REVISION #1	



**STORE NUMBER 6022**  
**6030 MARKET STREET**  
**WILMINGTON, NC 28405**

**PROJECT NO.:** 20-15081.02  
**DATE:** 11 NOV 16  
**SHEET TITLE:** DETAILS  
**SHEET NO.:** C-6.8

**PARAMOUNT ENGINEERING INC.**  
122 Cinema Drive  
Wilmington, North Carolina 28403  
(910) 791-6707 (O) (910) 791-6700 (F)  
NC License # C-2846



